

VALLEY VIEW LEGEND

EXISTING

RIGHT-OF-WAY
LOT LINE
PAVEMENT OR SIDEWALK
SANITARY MANHOLE
FIRE HYDRANT
GATE VALVE
CATCH BASIN
CURB AND GUTTER
SIGN
UTILITY POLE WITH LIGHT
GUY WIRE
SHRUB
CONIFEROUS OR DECIDUOUS TREE

PROPOSED

PROJECT LIMITS/STAGING LIMITS
FIRE HYDRANT AND GATE VALVE
CURB AND GUTTER
BITUMINOUS ROADWAY PAVEMENT
BITUMINOUS DRIVEWAY PAVEMENT
CONCRETE DRIVEWAY PAVEMENT
4" CONCRETE WALK
6" CONCRETE WALK
CLEAR SHRUB
CLEAR AND GRUB TREE

TRACY AVENUE LEGEND

EXISTING

RIGHT OF WAY
PERMANENT EASEMENT
PROPERTY LINE
HORIZONTAL CONTROL POINT
BENCHMARK
SURVEY MARKER
SOIL BORING
SANITARY SEWER AND MANHOLE
FORCE MAIN AND LIFT STATION
SANITARY SEWER SERVICE & CLEANOUT
WATER MAIN, HYDRANT, VALVE AND MANHOLE
WATER SERVICE AND CURB STOP BOX
STORM SEWER, MANHOLE AND CATCH BASIN
CULVERT AND APRON ENDWALL
GAS MAIN, VALVE, VENT AND METER
HANDHOLE
BURIED FIBER OPTIC CABLE AND MANHOLE
BURIED PHONE CABLE, PEDESTAL AND MANHOLE
BURIED TV CABLE, PEDESTAL AND MANHOLE
BURIED ELECTRIC CABLE, PEDESTAL, MANHOLE, TRANSFORMER AND METER
OVERHEAD WIRE, POLE AND GUY WIRE
LIGHT POLE
TRAFFIC SIGNAL
STREET NAME SIGN
SIGN (NON STREET NAME)
RAILROAD TRACKS
DECIDUOUS AND CONIFEROUS TREE
BUSH / SHRUB AND STUMP
EDGE OF WOODED AREA
WETLAND
BUILDING
FENCE (UNIDENTIFIED)
BARBED WIRE FENCE
CHAIN LINK FENCE
ELECTRIC WIRE FENCE
WOOD FENCE
WOVEN WIRE FENCE
PLATE BEAM GUARDRAIL
CABLE GUARDRAIL
POST / BOLLARD
RETAINING WALL

PROPOSED

STREET CENTERLINE
CONSTRUCTION LIMITS
STORM SEWER, MANHOLE AND CATCH BASIN
SIGN (NON STREET NAME)
CURB AND GUTTER
RIGHT-OF-WAY
INLET PROTECTION

CITY OF EDINA, MINNESOTA
2020 PEDESTRIAN AND BICYCLE IMPROVEMENTS



CONSTRUCTION PLAN FOR: MULTI-USE BITUMINOUS TRAIL, CONCRETE SIDEWALK, INTERSECTION IMPROVEMENTS

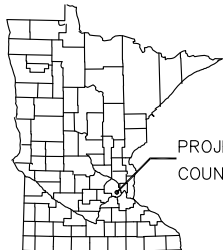
LOCATED ON: TRACY AVE BETWEEN VALLEY LN AND VALLEY VIEW RD AND AT CREEK VALLEY RD TO TRACY AVE ROUNDABOUT AND THE INTERSECTION OF GLEASON RD AND MCCAULEY TR

GOVERNING SPECIFICATIONS

THE 2018 EDITION OF THE MINNESOTA DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR CONSTRUCTION" SHALL GOVERN EXCEPT AS MODIFIED BY THE CITY OF EDINA SPECIFICATIONS FOR THIS PROJECT.

ALL TRAFFIC CONTROL DEVICES SHALL CONFORM AND BE INSTALLED IN ACCORDANCE TO THE "MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (MNMUTCD) AND PART VI, "FIELD MANUAL FOR TEMPORARY TRAFFIC CONTROL ZONE LAYOUTS."

THE SUBSURFACE UTILITY INFORMATION IN THIS PLAN IS QUALITY LEVEL D ACCORDING TO THE GUIDELINES OF CI/ASCE 38-02, "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA."



PROJECT LOCATION
COUNTY: HENNEPIN



Know what's below.
Call before you dig.

PLAN REVISIONS		
DATE	SHEET NO.	APPROVED BY

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THIS PLAN CONTAINS 32 SHEETS

LOCAL AGENCY SIGNATURES

DESIGN ENGINEER: I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION, AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA

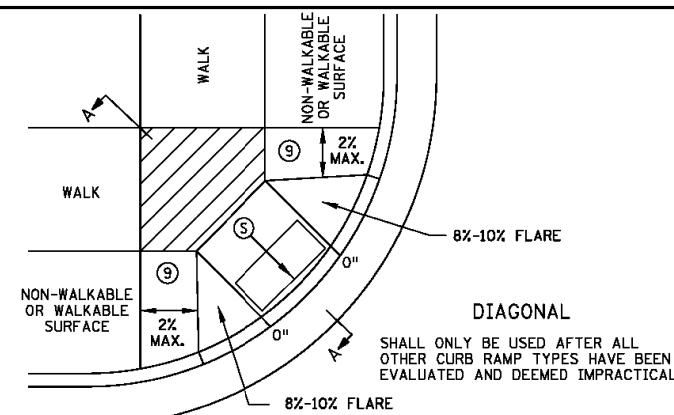
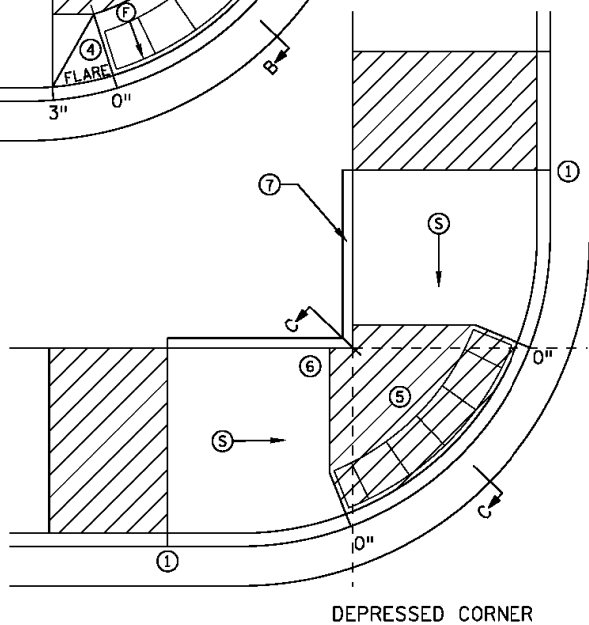
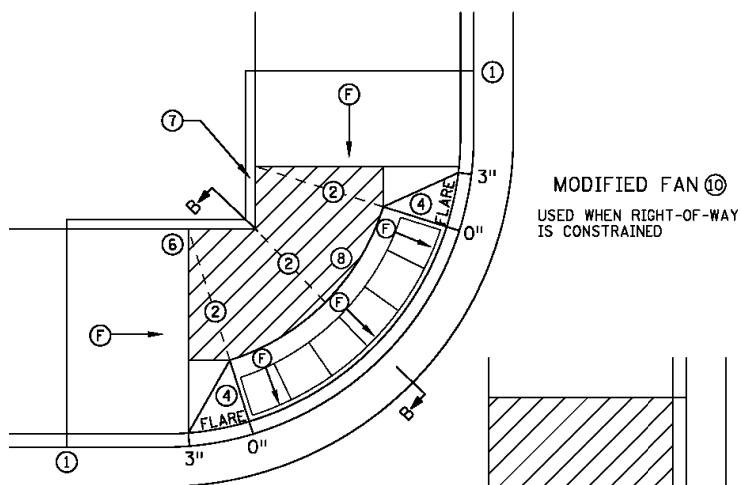
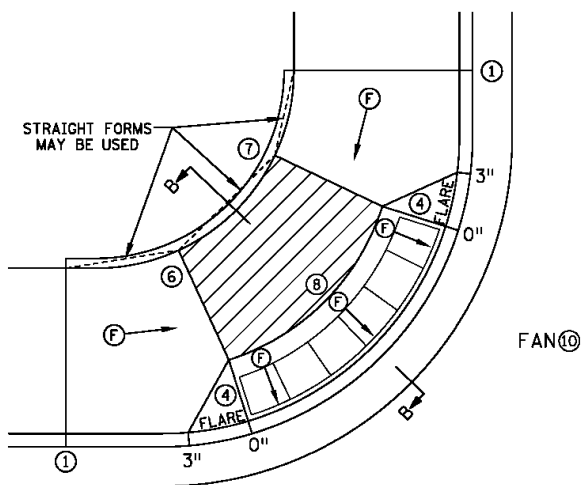
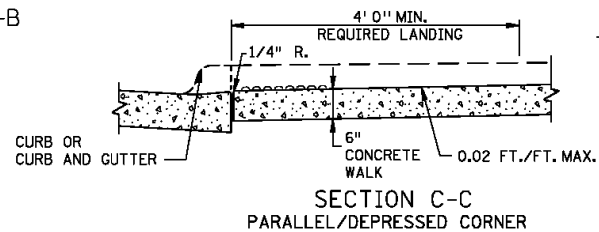
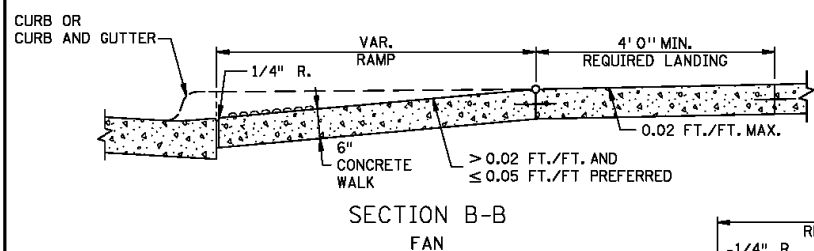
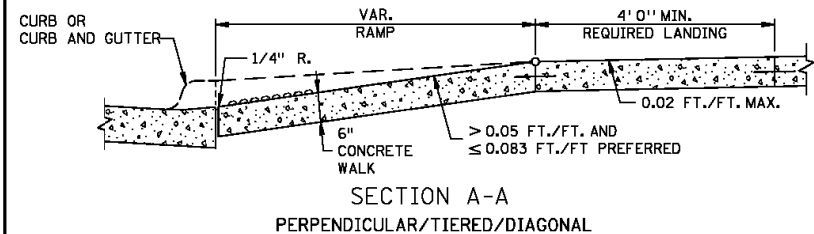
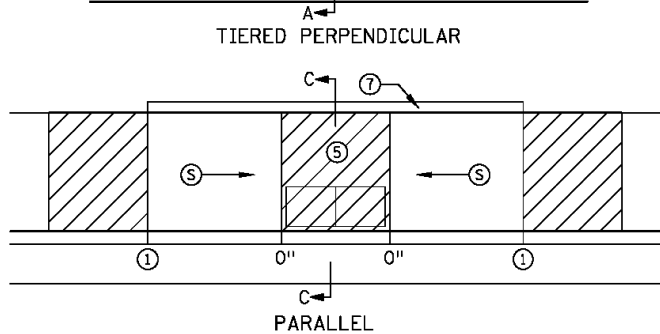
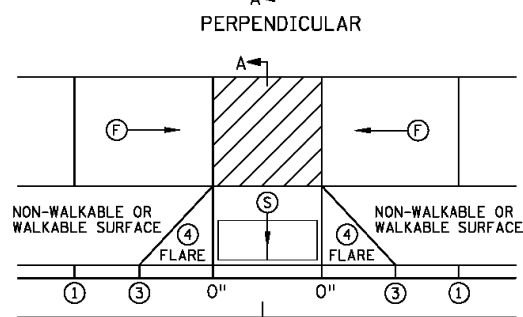
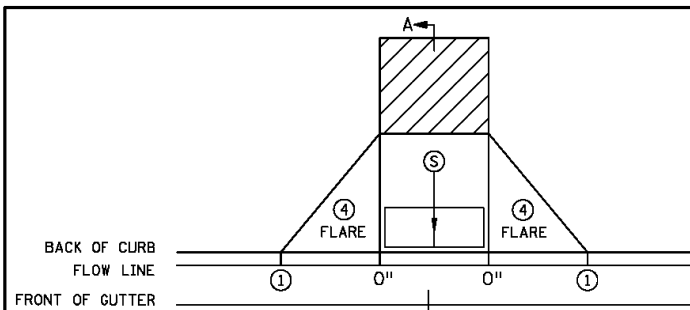
DATE: 06/22/20 LICENSE NO. 42003

SIGNATURE: Aaron Ditzler PRINTED NAME: AARON DITZLER

SIGNATURE: Chad A. Miller DATE: 06/22/20
APPROVED: CITY ENGINEER

EDINA IMP. NO. S-166, AND S-171

EDINA CONTRACT NO. ENG 20-13 SHEET 1 OF 32



NOTES:

LANDINGS SHALL BE LOCATED ANYWHERE THE PEDESTRIAN ACCESS ROUTE (PAR) CHANGES DIRECTION, AT THE TOP OF RAMPS THAT HAVE RUNNING SLOPES GREATER THAN 5.0%, AND IF THE APPROACHING WALK IS INVERSE GRADE GREATER THAN 2%.

INITIAL CURB RAMP LANDINGS SHALL BE CONSTRUCTED WITHIN 15' FROM THE BACK OF CURB, WITH 6' FROM THE BACK OF CURB BEING THE PREFERRED DISTANCE, ONLY APPLICABLE WHEN THE INITIAL RAMP RUNNING SLOPE IS OVER 5.0%.

SECONDARY CURB RAMP LANDINGS ARE REQUIRED FOR EVERY 30' OF VERTICAL RISE WHEN THE LONGITUDINAL RUNNING SLOPE IS GREATER THAN 5.0%.

CONTRACTION JOINTS SHALL BE CONSTRUCTED ALONG ALL GRADE BREAKS WITHIN THE PAR. 1/4" DEEP VISUAL JOINTS SHALL BE USED AT THE TOPS OF CONCRETE FLARES ADJACENT TO WALKABLE SURFACES.

ALL GRADE BREAKS WITHIN THE PAR SHALL BE PERPENDICULAR TO THE PATH OF TRAVEL, THUS BOTH SIDES OF A SLOPED WALKING SURFACE MUST BE EQUAL LENGTH, EXCEPT AS STATED IN (6) BELOW.

TO ENSURE INITIAL RAMPS AND INITIAL LANDINGS ARE PROPERLY CONSTRUCTED, LANDINGS SHALL BE CAST SEPARATELY, FOLLOW SIDEWALK REINFORCEMENT DETAILS ON SHEET 6 AND THE ADA SPECIAL PROVISIONS - PROSECUTION OF WORK (ADA).

TOP OF CURB SHALL MATCH PROPOSED ADJACENT WALK GRADE.

WHEN THE BOULEVARD IS 4' WIDE OR LESS, THE TOP OF CURB TAPER SHALL MATCH THE RAMP SLOPES TO REDUCE NEGATIVE BOULEVARD SLOPES FROM THE TOP BACK OF CURB TO THE PAR.

ALL RAMP TYPES SHOULD HAVE A MINIMUM 3' LONG RAMP LENGTH.

4' MINIMUM WIDTH OF DETECTABLE WARNING IS REQUIRED FOR ALL RAMPS. DETECTABLE WARNINGS SHALL CONTINUOUSLY EXTEND FOR A MIN. OF 24" IN THE PATH OF TRAVEL. DETECTABLE WARNING TO COVER ENTIRE WIDTH OF SHARED-USE PATHS AND THE ENTIRE PAR WIDTH OF THE WALK. DETECTABLE WARNING SHOULD BE 6" LESS THAN THE PAR/TRAIL WIDTH, ARC LENGTH OF RADIAL DETECTABLE WARNINGS SHOULD NOT BE GREATER THAN 20 FEET.

RECTANGULAR DETECTABLE WARNINGS SHALL BE SETBACK 3" FROM THE BACK OF CURB. RADIAL DETECTABLE WARNINGS SHALL BE SETBACK 3" MINIMUM TO 6" MAXIMUM FROM THE BACK OF CURB.

- 1 MATCH FULL HEIGHT CURB.
- 2 4' MINIMUM DEPTH LANDING REQUIRED ACROSS TOP OF RAMP.
- 3 3" HIGH CURB WHEN USING A 3' LONG RAMP, 4" HIGH CURB WHEN USING A 4' LONG RAMP.
- 4 SEE SHEET 4 OF 6, TYPICAL SIDE TREATMENT OPTIONS, FOR DETAILS ON FLARES AND RETURNED CURBS, WHEN INITIAL LANDING IS AT FULL CURB HEIGHT.
- 5 DETECTABLE WARNINGS MAY BE PART OF THE 4' X 4' MIN. LANDING AREA IF IT IS NOT FEASIBLE TO CONSTRUCT THE LANDING OUTSIDE OF THE DETECTABLE WARNING AREA.
- 6 THE GRADE BREAK SHALL BE PERPENDICULAR TO THE BACK OF WALK. THIS WILL ENSURE THAT THE GRADE BREAK IS PERPENDICULAR TO THE DIRECTION OF TRAVEL (TYPICAL FOR ALL).
- 7 WHEN ADJACENT TO GRASS, GRADING SHALL ALWAYS BE USED WHEN FEASIBLE. V CURB, IF USED, SHALL BE PLACED OUTSIDE THE SIDEWALK LIMITS WHEN RIGHT OF WAY ALLOWS. WHEN ADJACENT TO PARKING LOTS, CONCRETE OR BITUMINOUS TAPERS SHOULD BE USED OVER V CURB TO REDUCE TRIPPING HAZARDS AND FACILITATE SNOW & ICE REMOVAL.
- 8 A 7' MIN TOP RADIUS GRADE BREAK REQUIRED TO BE CONSTRUCTIBLE.
- 9 PAVE FULL WALK WIDTH.
- 10 "S" SLOPES ON FANS SHALL ONLY BE USED WHEN ALL OTHER FEASIBLE OPTIONS HAVE BEEN EVALUATED AND DEEMED IMPRACTICAL.

LEGEND	
(S)	INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE BETWEEN 5.0% MINIMUM AND 8.3% MAXIMUM IN THE DIRECTION SHOWN AND THE CROSS SLOPE SHALL NOT EXCEED 2.0%.
(F)	INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE GREATER THAN 2.0% AND LESS THAN 5.0% IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%.
[Hatched Box]	LANDING AREA - 4' X 4' MIN. (5' X 5' MIN. PREFERRED) DIMENSIONS AND MAX 2.0% SLOPE IN ALL DIRECTIONS. LANDING SHALL BE FULL WIDTH OF INCOMING PAR.
X"	CURB HEIGHT

REVISION:

APPROVED: JANUARY 23, 2017

OPERATIONS ENGINEER

MINNESOTA DEPARTMENT OF TRANSPORTATION

STATE DESIGN ENGINEER

1-23-2017

PEDESTRIAN CURB RAMP DETAILS

STANDARD PLAN 5-297.250

1 OF 6

NO.	DATE	BY	REMARKS

PEDESTRIAN CURB RAMP DETAILS

CITY OF EDINA

7450 METRO BOULEVARD

EDINA, MN 55439-3037

Ph: 952-826-0371

2020 PEDESTRIAN AND BICYCLE IMPROVEMENTS

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER IN THE STATE OF MINNESOTA

DATE: 06/22/20 LIC. NO. 42003

CJG

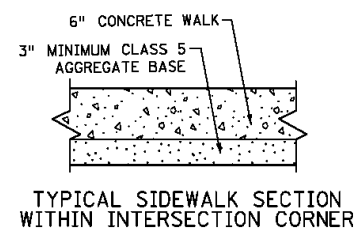
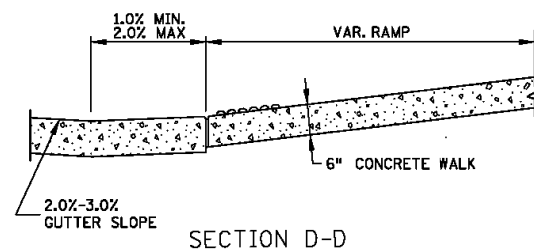
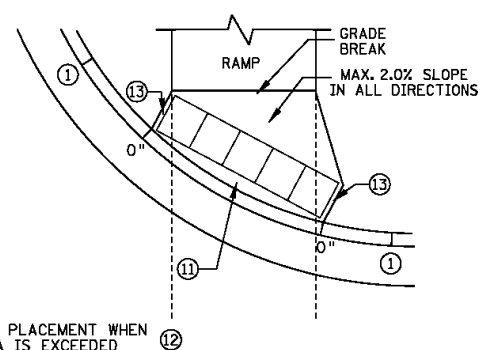
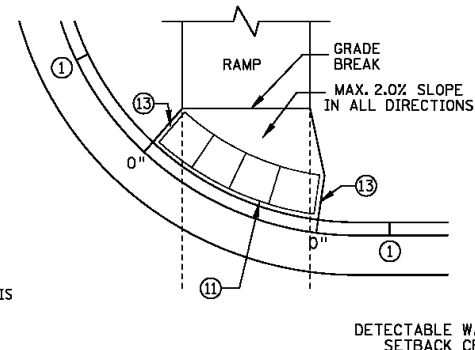
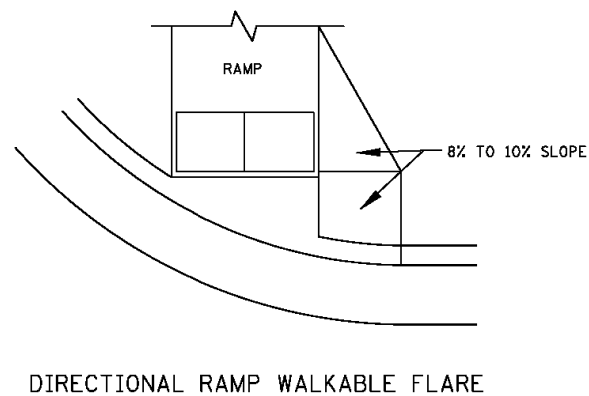
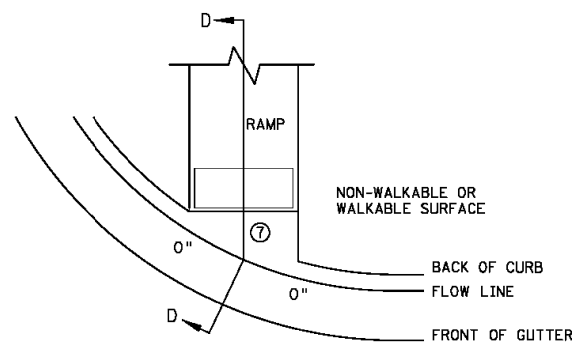
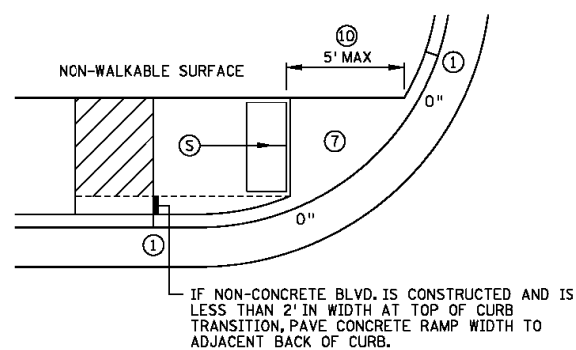
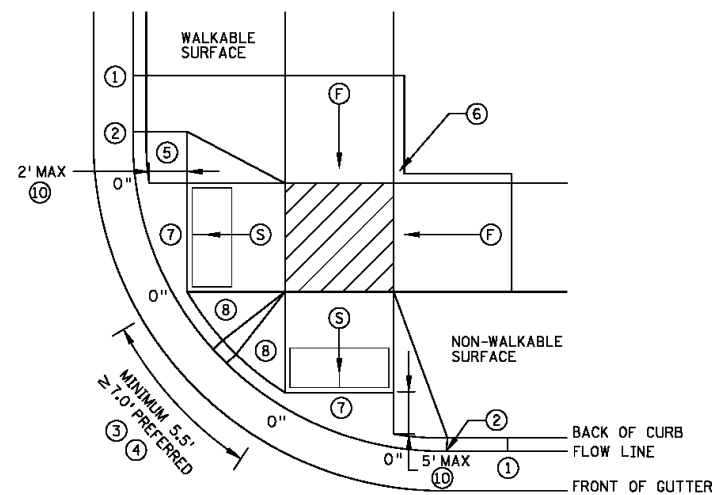
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ATD

06/22/20

CONTR.# ENG 20-13

SHEET 3 OF 32



- NOTES:

LANDINGS SHALL BE LOCATED ANYWHERE THE PEDESTRIAN ACCESS ROUTE (PAR) CHANGES DIRECTION, AT THE TOP OF RAMPS THAT HAVE RUNNING SLOPES GREATER THAN 5.0%, AND IF THE APPROACHING WALK IS INVERSE GRADE.

INITIAL CURB RAMP LANDINGS SHALL BE CONSTRUCTED WITHIN 15' FROM THE BACK OF CURB, WITH 6' FROM THE BACK OF CURB BEING THE PREFERRED DISTANCE, ONLY APPLICABLE WHEN THE INITIAL RAMP RUNNING SLOPE IS OVER 5.0%.

SECONDARY CURB RAMP LANDINGS ARE REQUIRED FOR EVERY 30" OF VERTICAL RISE WHEN THE LONGITUDINAL SLOPE IS GREATER THAN 5.0%.

CONTRACTION JOINTS SHALL BE CONSTRUCTED ALONG ALL GRADE BREAKS WITHIN THE PAR. 1/4" DEEP VISUAL JOINTS SHALL BE USED AT THE TOP GRADE BREAK OF CONCRETE FLARES ADJACENT TO WALKABLE SURFACES.

ALL GRADE BREAKS WITHIN THE PAR SHALL BE PERPENDICULAR TO THE PATH OF TRAVEL.
THUS BOTH SIDES OF A SLOPED WALKING SURFACE MUST BE EQUAL LENGTH.

TO ENSURE INITIAL RAMPS AND INITIAL LANDINGS ARE PROPERLY CONSTRUCTED, LANDINGS SHALL BE CAST SEPARATELY. FOLLOW SIDEWALK REINFORCEMENT DETAILS ON SHEET 6 AND THE ADA SPECIAL PROVISION (PROSECUTION OF WORK).

TOP OF CURB SHALL MATCH PROPOSED ADJACENT WALK GRADE.

WHEN THE BOULEVARD IS 4' WIDE OR LESS, THE TOP OF CURB TAPER SHALL MATCH THE RAMP SLOPES TO REDUCE NEGATIVE BOULEVARD SLOPES FROM THE TOP BACK OF CURB TO THE PAR.

ALL RAMP TYPES SHOULD HAVE A MINIMUM 3' LONG RAMP LENGTH.

4' MINIMUM WIDTH OF DETECTABLE WARNING IS REQUIRED FOR ALL RAMPS. DETECTABLE WARNINGS SHALL CONTINUOUSLY EXTEND FOR A MIN. OF 24" IN THE PATH OF TRAVEL. DETECTABLE WARNING TO COVER ENTIRE WIDTH OF SHARED-USE PATH AND THE ENTIRE PAR WIDTH OF THE WALK. DETECTABLE WARNING SHOULD BE 6" LESS THAN THE PAR/PATH WIDTH. ARC LENGTH OF RADIAL DETECTABLE WARNINGS SHOULD NOT BE GREATER THAN 20 FEET.

RADIAL DETECTABLE WARNINGS SHALL BE SETBACK 3" MINIMUM TO 6" MAXIMUM FROM THE BACK OF CURB.
SEE NOTES (a) & (b) FOR INFORMATION REGARDING RECTANGULAR DETECTABLE WARNING PLACEMENT.

- ① MATCH FULL CURB HEIGHT.
- ② 3" HIGH CURB WHEN USING A 3' LONG RAMP
4" HIGH CURB WHEN USING A 4' LONG RAMP.
- ③ 3" MINIMUM CURB HEIGHT (5.5' MIN. DISTANCE REQUIRED BETWEEN DOMES)
4" PREFERRED (7' MIN. DISTANCE REQUIRED BETWEEN DOMES).
- ④ THE "BUMP" IN BETWEEN THE RAMPS SHOULD NOT BE IN THE PATH OF TRAVEL FOR COMBINED DIRECTIONAL RAMPS. IF THIS OCCURS MODIFY THE RAMP LOCATION OR SWITCH RAMP TO A FAN/DEPRESSED CORNER.
- ⑤ WHEN USING CONCRETE PAVED FLARES ON THE OUTSIDE OF DIRECTIONAL RAMPS, AND ADJACENT TO A WALKABLE SURFACE, DIRECTIONAL RAMP FLARES SHOULD BE USED. SEE THE DETAIL ON THIS SHEET.
- ⑥ GRADING SHALL ALWAYS BE USED WHEN FEASIBLE. V CURB, IF USED, SHALL BE PLACED OUTSIDE THE SIDEWALK LIMITS WHEN RIGHT OF WAY ALLOWS. WHEN ADJACENT TO PARKING LOTS, CONCRETE OR BITUMINOUS TAPERS SHOULD BE USED OVER V CURB TO REDUCE TRIPPING HAZARDS AND FACILITATE SNOW & ICE REMOVAL.
- ⑦ MAX. 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK AND DRAIN TO FLOW LINE. SHALL BE CONSTRUCTED INTEGRAL WITH CURB AND GUTTER.
- ⑧ 8% TO 10% WALKABLE FLARE.
- ⑨ PLACE DOMES AT THE BACK OF CURB WHEN ALLOWABLE SETBACK CRITERIA IS EXCEEDED.
- ⑩ FRONT EDGE OF DETECTABLE WARNING SHALL BE SET BACK 2' MAXIMUM WHEN ADJACENT TO WALKABLE SURFACE, AND 5' MAXIMUM WHEN ADJACENT TO NON-WALKABLE SURFACE WITH ONE CORNER SET 3" FROM BACK OF CURB. A WALKABLE SURFACE IS DEFINED AS A PAVED SURFACE ADJACENT TO A CURB RAMP WITHOUT RAISED OBSTACLES THAT COULD MISTAKENLY BE TRAVERSED BY A USER WHO IS VISUALLY IMPAIRED.
- ⑪ RECTANGULAR DETECTABLE WARNINGS ARE TO BE SETBACK UP TO 9" FROM THE BACK OF CURB WITH CORNERS SET 3" FROM BACK OF CURB. IF 9" SETBACK IS EXCEEDED USE RADIAL DETECTABLE WARNINGS.
- ⑫ FOR DIRECTIONAL RAMPS WITH THE DETECTABLE WARNINGS PLACED AT THE BACK OF CURB, THE DETECTABLE WARNINGS SHALL COVER THE ENTIRE WIDTH OF THE WALK/PATH. THIS ENSURES A DETECTABLE EDGE AND HELPS ELIMINATE THE CURB TAPER OBSTRUCTING THE PATH OF PEDESTRIAN TRAVEL.
- ⑬ THE CONCRETE WALK SHALL BE FORMED AND CONSTRUCTED PERPENDICULAR TO THE BACK OF CURB. MAINTAIN 3" BETWEEN EDGE OF DOMES AND EDGE OF CONCRETE.
- ⑭ TO BE USED FOR ALL DIRECTIONAL RAMPS, EXCEPT WHERE DOMES ARE PLACED ALONG THE BACK OF CURB.

REVISION:

APPROVED: JANUARY 23, 2017

Adam Zehr

OPERATIONS ENGINEER

CURB FOR DIRECTIONAL RAMPS (14)



Tom Slin

REVISÉ:

APPROVED

1-23-2017

PEDESTRIAN CURB RAMP DETAILS

STANDARD PLAN 5-297.250

2 OF 6

[illegible]

PEDESTRIAN CURB RAMP DETAILS

CITY OF EDINA

7450 METRO BOULEVARD
EDINA, MN 55439-3037
Ph: 952-826-0371



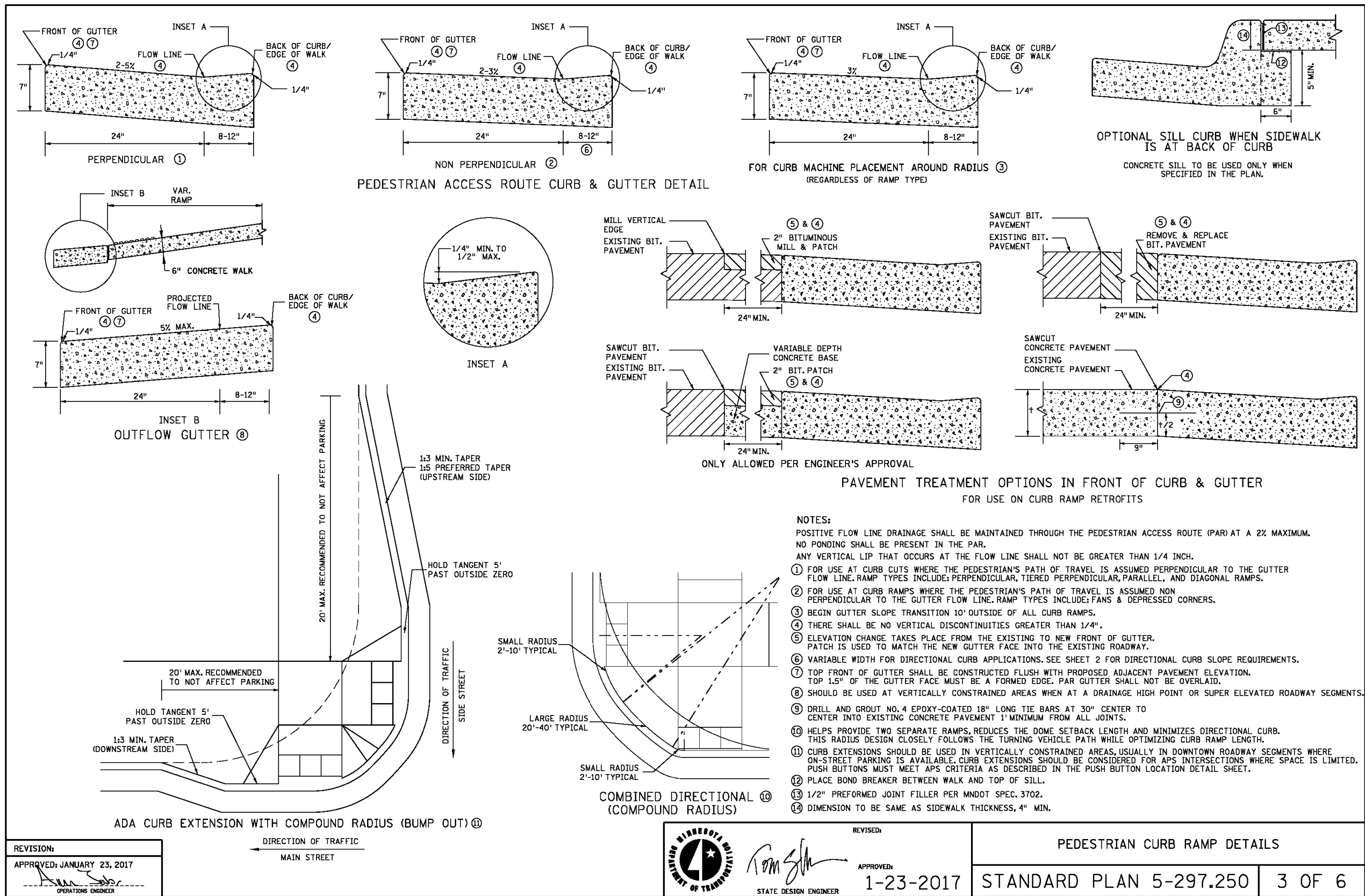
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AND THAT I AM A DULY
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UNDER THE LAWS OF THE STATE
OF MINNESOTA


Carrey Z. Hoff

DATE: 06/22/20 LIC. NO. 42003

DRAWN:	CJG
CHECKED:	CAM
APPROVED:	ATD
DATE:	06/22/20
CONTR.#	ENG 20-13
SHEET	4 OF 32



REVISION:
APPROVED: JANUARY 23, 2017
OPERATIONS ENGINEER



MINNESOTA
DEPARTMENT OF
TRANSPORTATION

REVISOR:

1-23-2017


APPROVED:

STATE DESIGN ENGINEER

PEDESTRIAN CURB RAMP DETAILS	
STANDARD PLAN 5-297.250	3 OF 6

PEDESTRIAN CURB
RAMP DETAILS

CITY OF EDINA
7450 METRO BOULEVARD
EDINA, MN 55439-3037
Ph: 952-826-0371



2020 PEDESTRIAN AND
BICYCLE IMPROVEMENTS

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DATE: 06/22/20

LIC. NO. 42003

DRAWN: C/JG

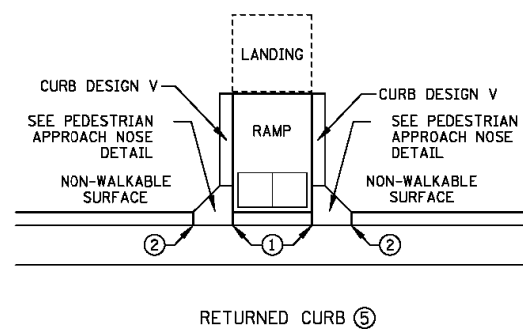
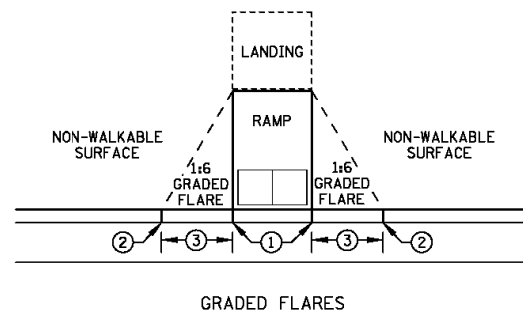
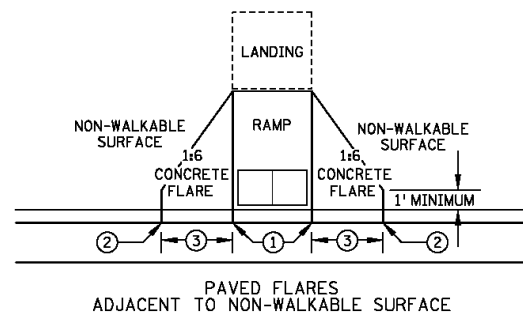
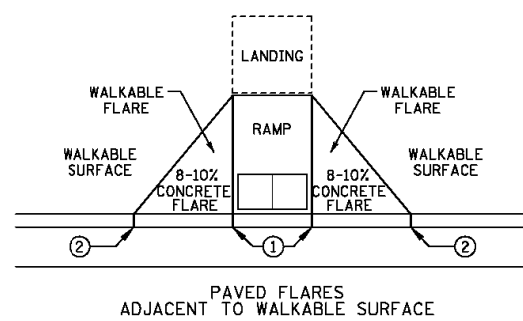
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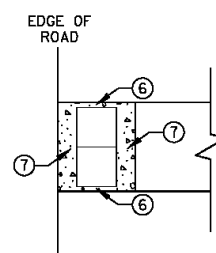
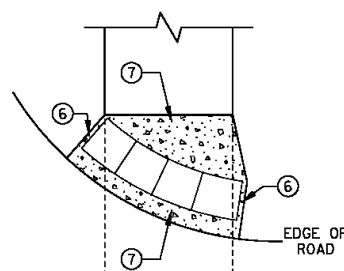
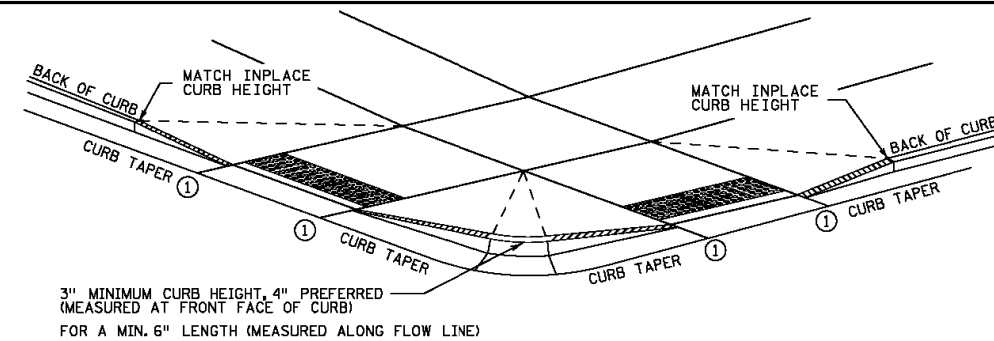
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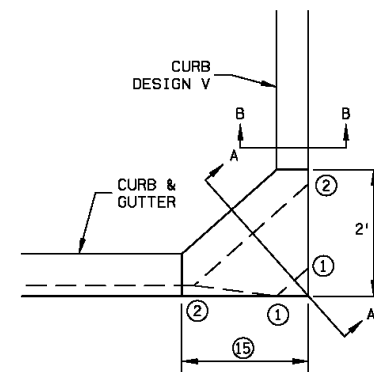
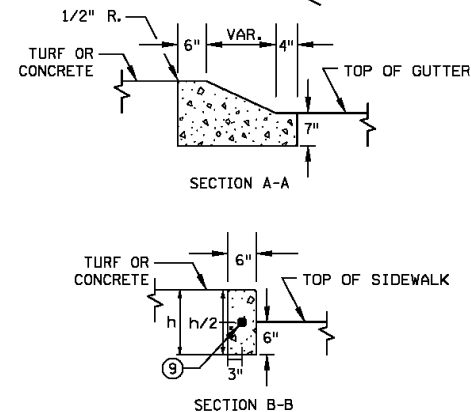
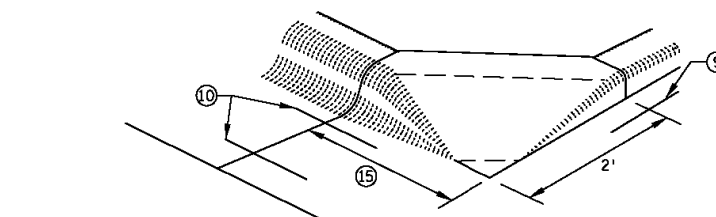
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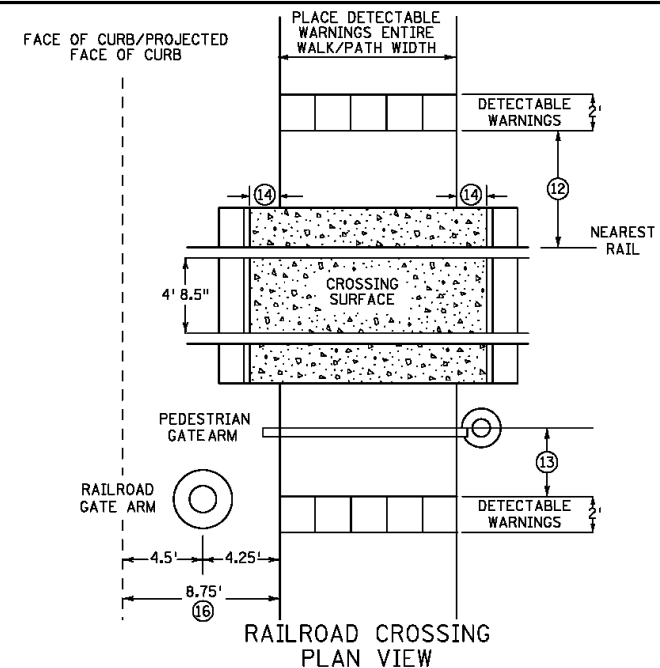
TYPICAL SIDE TREATMENT OPTIONS ④ ⑪



DETECTABLE EDGE WITHOUT CURB AND GUTTER



PEDESTRIAN APPROACH
NOSE DETAIL
(FOR RETURNED CURB
SIDE TREATMENT)



NOTES:

SEE STANDARD PLATE 7038 AND THIS SHEET FOR ADDITIONAL DETAILS ON DETECTABLE WARNING.
A WALKABLE SURFACE IS DEFINED AS A PAVED SURFACE ADJACENT TO A CURB RAMP WITHOUT
RAISED OBSTACLES THAT COULD MISTAKENLY BE TRAVERSED BY A USER WHO IS VISUALLY IMPAIRED.
CONCRETE FLARE LENGTHS ADJACENT TO NON-WALKABLE SURFACES SHOULD BE LESS THAN 8' LONG
MEASURED ALONG THE RAMPS FROM THE BACK OF CURB.

- ① 0" CURB HEIGHT.
- ② FULL CURB HEIGHT.
- ③ 2' FOR 4" HIGH CURB AND 3' FOR 6" HIGH CURB.
- ④ SIDE TREATMENTS ARE APPLICABLE TO ALL RAMP TYPES AND SHOULD BE IMPLEMENTED AS NEEDED AS FIELD CONDITIONS DICTATE. THE ENGINEER SHALL DETERMINE THE RAMP SIDE TREATMENTS BASED ON MAINTENANCE OF BOTH ROADWAY AND SIDEWALK, ADJACENT PROPERTY CONSIDERATIONS, AND MITIGATING CONSTRUCTION IMPACTS.
- ⑤ TYPICALLY USED FOR MEDIANS AND ISLANDS.
- ⑥ WHEN NO CONCRETE FLARES ARE PROPOSED, THE CONCRETE WALK SHALL BE FORMED AND CONSTRUCTED PERPENDICULAR TO THE EDGE OF ROADWAY, MAINTAIN 3" MAX. BETWEEN EDGE OF DOMES AND EDGE OF CONCRETE.
- ⑦ IF NO CURB AND GUTTER IS PLACED IN RURAL SECTIONS, DETECTABLE WARNINGS SHALL BE PLACED 1' FROM THE EDGE OF BITUMINOUS ROADWAY AND/OR BITUMINOUS SHARED-USE PATH TO PROVIDE VISUAL CONTRAST.
- ⑧ ALL CONSTRUCTED CURBS MUST HAVE A CONTINUOUS DETECTABLE EDGE FOR THE VISUALLY IMPAIRED. THIS DETECTABLE EDGE REQUIRES DETECTABLE WARNINGS WHEREVER THERE IS ZERO-INCH HIGH CURB. CURB TAPERS ARE CONSIDERED A DETECTABLE EDGE WHEN THE TAPER STARTS WITHIN 3" OF THE EDGE OF THE DETECTABLE WARNINGS AND UNIFORMLY RISES TO A 3-INCH MINIMUM CURB HEIGHT. ANY CURB NOT PART OF A CURB TAPER AND LESS THAN 3 INCHES IN HEIGHT IS NOT CONSIDERED A DETECTABLE EDGE AND THEREFORE IS NOT COMPLIANT WITH ACCESSIBILITY STANDARDS.
- ⑨ DRILL AND GROUT 1 - NO. 4 12" LONG REINFORCEMENT BAR (EPOXY COATED) WITH 3" MIN. COVER. REINFORCEMENT BARS ARE NOT NEEDED IF THE APPROACH NOSE IS POURED INTEGRAL WITH THE V CURB.
- ⑩ DRILL AND GROUT 2 - NO. 4 12" LONG REINFORCEMENT BARS (EPOXY COATED) WITH 3" MIN. COVER. REINFORCEMENT BARS ARE NOT NEEDED IF THE APPROACH NOSE IS POURED INTEGRAL WITH THE CURB AND GUTTER.
- ⑪ SIDE TREATMENT EXAMPLES SHOWN ARE WHEN THE INITIAL LANDING IS APPROXIMATELY LEVEL WITH THE FULL HEIGHT CURB (I.E. 6" LONG RAMP FOR 6" HIGH CURB). WHEN THE INITIAL LANDING IS MORE THAN 1" BELOW FULL HEIGHT CURB REFER TO SHEETS 1 & 2 TO MODIFY THE CURB HEIGHT TAPERS AND MAINTAIN POSITIVE BOULEVARD DRAINAGE.
- ⑫ NEAREST EDGE OF DETECTABLE WARNING SURFACES SHALL BE PLACED 12' MINIMUM TO 15' MAXIMUM FROM THE NEAREST RAIL. FOR SKEWED RAILWAYS IN NO INSTANCE SHALL THE DETECTABLE WARNING BE CLOSER THAN 12' MEASURED PERPENDICULAR TO THE NEAREST RAIL.
- ⑬ WHEN PEDESTRIAN GATES ARE PROVIDED, DETECTABLE WARNING SURFACES SHALL BE PLACED ON THE SIDE OF THE GATES OPPOSITE THE RAIL, 2' FROM THE APPROACHING SIDE OF THE GATE ARM. THIS CRITERIA GOVERNS OVER NOTE ⑫.
- ⑭ CROSSING SURFACE SHALL EXTEND 2' MINIMUM PAST THE OUTSIDE EDGE OF WALK OR SHARED-USE PATH.
- ⑮ 3' FOR MEDIANS AND SPLITTER ISLANDS. NOSE CAN BE REDUCED TO 2' ON FREE RIGHT ISLANDS.
- ⑯ SIDEWALK TO BE PLACED 8.75' MIN. FROM THE FACE OF CURB/PROJECTED FACE OF CURB. THIS ENSURES MIN. CLEARANCE BETWEEN THE SIDEWALK AND GATE ARM COUNTERWEIGHT SUPPORTS.

REVISION:

APPROVED: JANUARY 23, 2017

Adam Zohr

OPERATIONS ENGINEER



Tom Slin

REVISED:

APPROVED

1-23-2017

PEDESTRIAN CURB RAMP DETAILS

STANDARD PLAN 5-297.250

4 OF 6

PEDESTRIAN CURB RAMP DETAILS

CITY OF EDINA
7450 METRO BOULEVARD
EDINA, MN 55439-3037
Ph: 952-826-0371



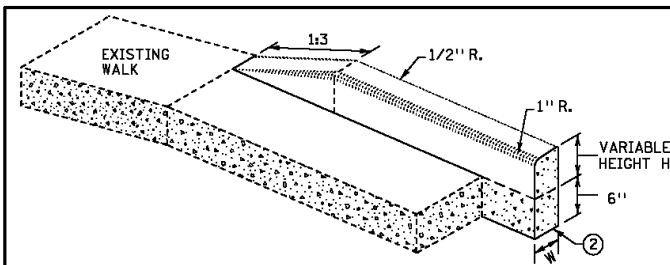
2020 PEDESTRIAN AND BICYCLE IMPROVEMENTS

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AND THAT I AM A DULY
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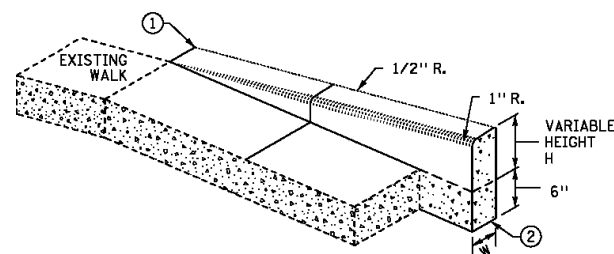
Carrey Z. Hoff

DATE: 06/22/20 LIC. NO. 42003

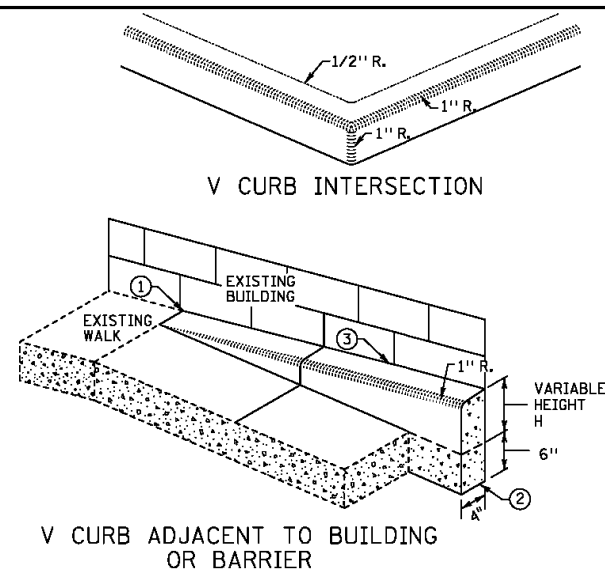
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CONTR.#	ENG 20-13
SHEET	6 OF 32



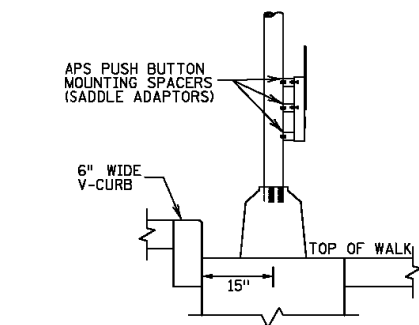
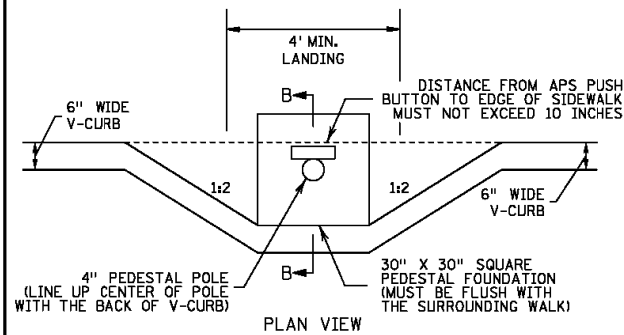
V CURB ADJACENT TO LANDSCAPE
CURB WITHIN SIDEWALK LIMITS



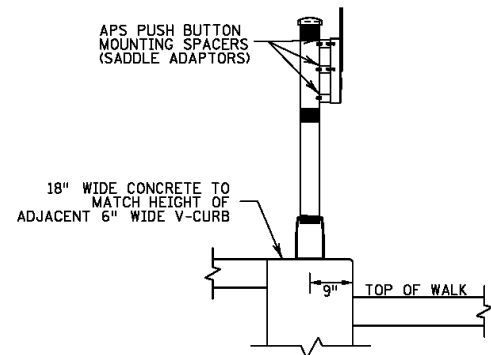
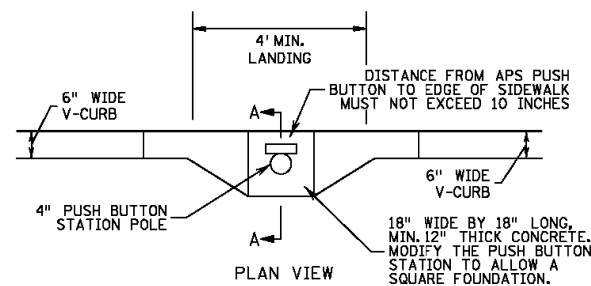
V CURB ADJACENT TO LANDSCAPE
CURB OUTSIDE SIDEWALK LIMITS



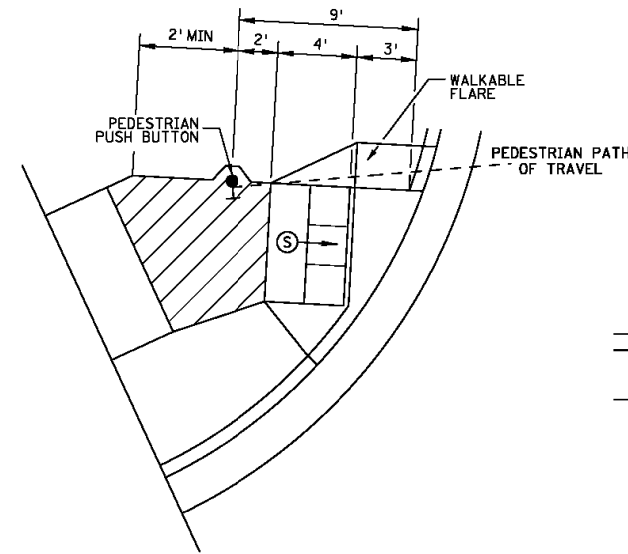
CONCRETE CURB DESIGN V	
CURB HEIGHT H	CURB WIDTH W
< 6"	4"
≥ 6"	6"



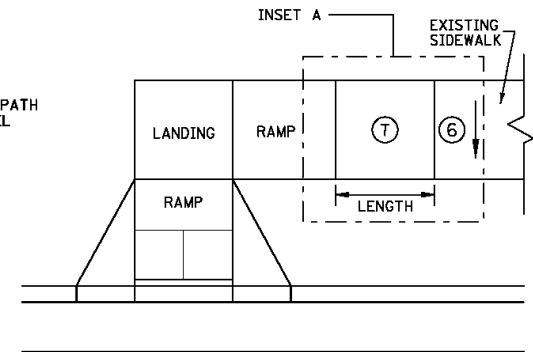
SECTION B-B
SIGNAL PEDESTAL & PUSH BUTTON (V-CURB)



SECTION A-A
PUSH BUTTON STATION (V-CURB)



SEMI-DIRECTIONAL RAMP (3,4,9)
3' DOME SETBACK, 4' LONG RAMP AND
PUSH BUTTON 9' FROM THE BACK OF CURB
PRIMARILY USED FOR APS APPLICATIONS
WHERE THE PAR DOES NOT CONTINUE PAST
THE PUSH BUTTON (DEAD-END SIDEWALK)



TRANSITION PANEL (4,5)

NOTES:

- A WALKABLE FLARE IS AN 8-10% CONCRETE FLARE THAT IS REQUIRED WHEN THE FLARE IS ADJACENT TO A WALKABLE SURFACE, OR WHEN THE PEDESTRIAN PATH OF TRAVEL OF A PUSH BUTTON TRAVERSES THE FLARE.
- ALL V CURB CONTRACTION JOINTS SHALL MATCH CONCRETE WALK JOINTS.
- WHERE RIGHT-OF-WAY ALLOWS, USE OF V CURB SHOULD BE MINIMIZED. GRADING ADJACENT TURF OR SLOPING ADJACENT PAVEMENT IS PREFERRED.
- V CURB SHALL BE PLACED OUTSIDE THE SIDEWALK LIMITS WHEN RIGHT OF WAY ALLOWS.
- V CURB NEXT TO BUILDING SHALL BE A 4" WIDTH AND SHALL MATCH PREVIOUS TOP OF SIDEWALK ELEVATIONS.
- END TAPERS AT TRANSITION SECTION SHALL MATCH INPLACE SIDEWALK GRADES.
- ALL V CURB SHALL MATCH BOTTOM OF ADJACENT WALK.
- EDGE BETWEEN NEW V CURB AND INPLACE STRUCTURE SHALL BE SEALED AND BOND BREAKER SHALL BE USED BETWEEN EXISTING STRUCTURE AND PLACED V-CURB.
- THE MAX. RATE OF CROSS SLOPE TRANSITIONING IS 1' LINEAR FOOT OF SIDEWALK PER HALF PERCENT CROSS SLOPE. WHEN PAR WIDTH IS GREATER THAN 6' OR THE RUNNING SLOPE IS GREATER THAN 5%, DOUBLE THE CALCULATED TRANSITION LENGTH.
- TRANSITION PANEL(S) ARE TO ONLY BE USED AFTER THE RAMP, OR IF NEEDED, LANDING ARE AT THE FULL CURB HEIGHT (TYPICAL SECTION).
- EXISTING CROSS SLOPE GREATER THAN 2.0%.

LEGEND	
Ⓢ	THESE LONGITUDINAL SLOPE RANGES SHALL BE THE STARTING POINT, IF SITE CONDITIONS WARRANT, LONGITUDINAL SLOPES UP TO 8.3% OR FLATTER ARE ALLOWED.
↓	INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE BETWEEN 5.0% MINIMUM AND 8.3% MAXIMUM IN THE DIRECTION SHOWN AND THE CROSS SLOPE SHALL NOT EXCEED 2.0%.
▨	LANDING AREA - 4' X 4' MIN. (5' X 5' MIN. PREFERRED) DIMENSIONS AND MAX 2.0% SLOPE IN ALL DIRECTIONS. LANDING SHALL BE FULL WIDTH OF INCOMING PAR.
①	TRANSITION PANEL(S) - TO BE USED FOR TRANSITIONING THE CROSS-SLOPE OF A RAMP TO THE EXISTING WALK CROSS-SLOPE. RATE OF TRANSITION SHOULD BE 0.5% PER 1 LINEAR FOOT OF WALK. SEE THIS SHEET FOR ADDITIONAL INFORMATION.

REVISION:
APPROVED: JANUARY 23, 2017
OPERATIONS ENGINEER

REVISED:	APPROVED:
1-23-2017	STATE DESIGN ENGINEER

PEDESTRIAN CURB RAMP DETAILS	
STANDARD PLAN 5-297.250	5 OF 6

NO.	DATE	BY	REMARKS

PEDESTRIAN CURB RAMP DETAILS

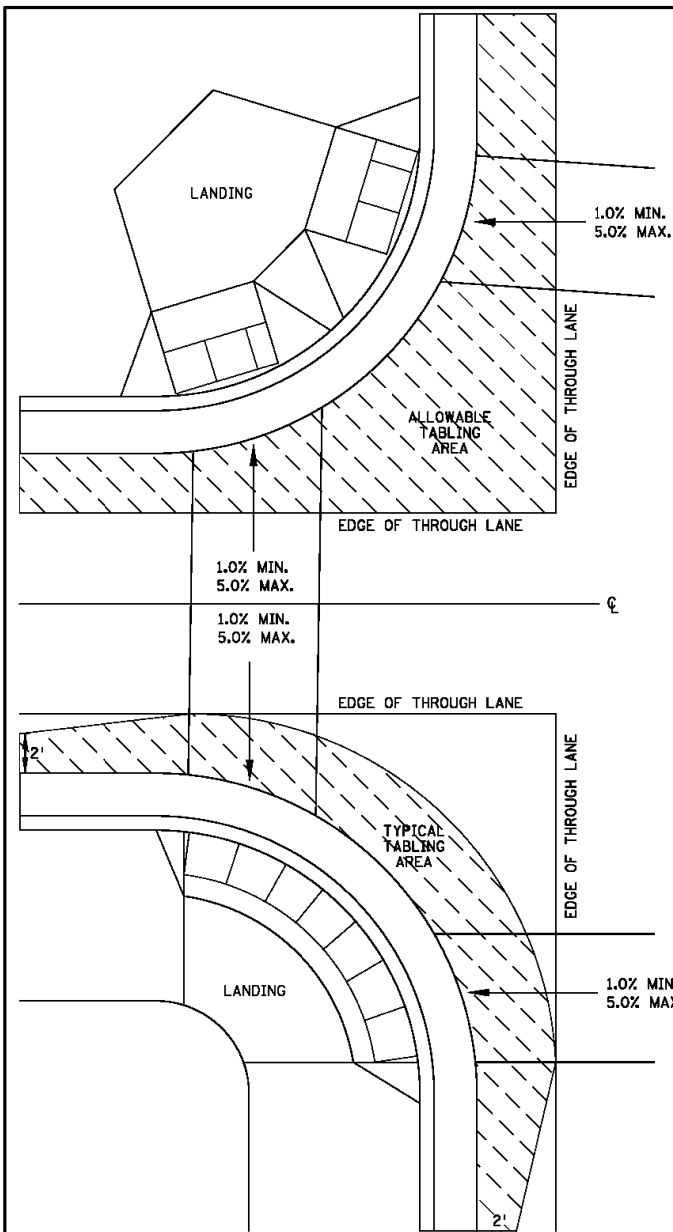
CITY OF EDINA
7450 METRO BOULEVARD
EDINA, MN 55439-3037
Ph: 952-826-0371

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SHEET 7 OF 32



CURB LINE AND ROAD CROSSING ADJUSTMENTS

"TABLING" OF CROSSWALKS MEANS MAINTAINING LESS THAN 2% CROSS SLOPE WITHIN A CROSSWALK, IS REQUIRED WHEN A ROADWAY IS IN A STOP OR YIELD CONDITION AND THE PROJECT SCOPE ALLOWS.

RECONSTRUCTION PROJECTS: ON FULL PAVEMENT REPLACEMENT PROJECTS "TABLING" OF ENTIRE CROSSWALK SHALL OCCUR WHEN FEASIBLE.

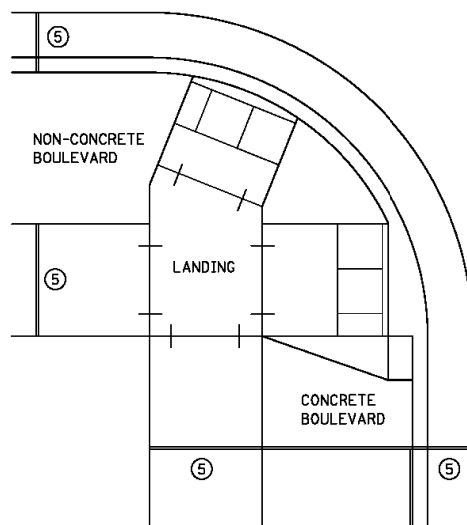
MILL & OVERLAY PROJECTS: "TABLING" OF FLOW LINES, IN FRONT OF THE PEDESTRIAN RAMP, IS REQUIRED WHEN THE EXISTING FLOW LINE IS GREATER THAN 2%. WARPING OF THE BITUMINOUS PAVEMENT CAN NOT EXTEND INTO THE THROUGH LANE. TABLE THE FLOW LINE TO 2% OR AS MUCH AS POSSIBLE WHILE ADHERING TO THE FOLLOWING CRITERIA:

- 1) 1.0% MIN. CROSS-SLOPE OF THE ROAD
- 2) 5.0% MAX. CROSS-SLOPE OF THE ROAD
- 3) "TABLE" FLOW LINE UP TO 4% CHANGE FROM EXISTING SLOPE IN FRONT OF PEDESTRIAN RAMP
- 4) UP TO 2% CHANGE IN FLOW LINE FROM EXISTING SLOPE BEYOND THE PEDESTRIAN CURB RAMP

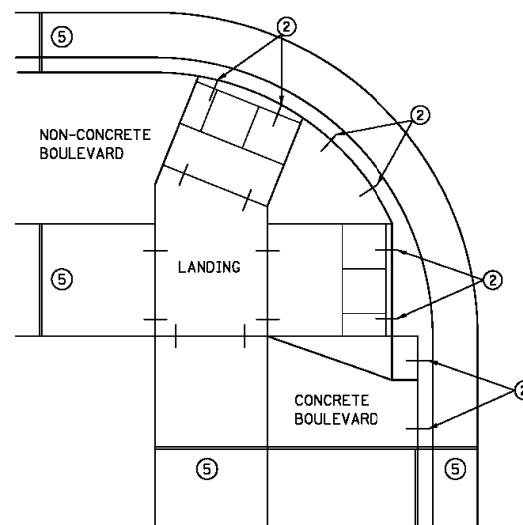
STAND-ALONE ADA RETROFITS: FOLLOW MILL & OVERLAY CRITERIA ABOVE HOWEVER ALL PAVEMENT WARPING IS DONE WITH BITUMINOUS PATCHING ON BITUMINOUS ROADWAYS AND FULL-DEPTH APRON REPLACEMENT ON CONCRETE ROADWAYS.

RAISING OF CURB LINES SHOULD OCCUR IN VERTICALLY CONSTRAINED AREAS. RAISE THE CURB LINES ENOUGH TO ALLOW COMPLIANT RAMPS OR AS MUCH AS POSSIBLE WHILE ADHERING TO THE FOLLOWING CRITERIA:

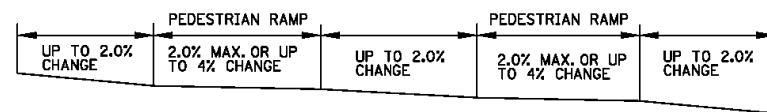
- 1) 1.0% MIN. AND 5.0% MAXIMUM CROSS-SLOPE OF THE ROAD
- 2) 1.0% MIN. FLOW LINE (ON EITHER SIDE OF PEDESTRIAN RAMP) TO MAINTAIN POSITIVE DRAINAGE
- 3) 5.0% RECOMMENDED MAX. FLOW LINE
- 4) LONGITUDINAL THROUGH LANE ROADWAY TAPERS SHOULD BE 1" VERTICAL PER 15' HORIZONTAL



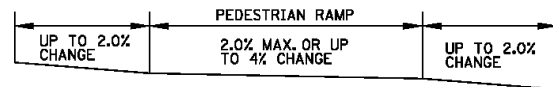
EXPANSION MATERIAL PLACEMENT FOR CONCRETE AND BITUMINOUS ROADWAYS



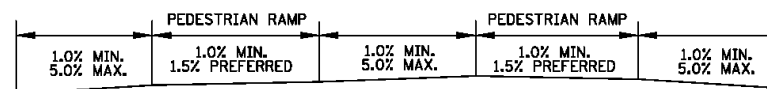
OPTIONAL CURB LINE REINFORCEMENT PLACEMENT ON BITUMINOUS ROADWAYS ④



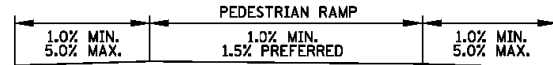
FLOW LINE PROFILE "TABLE" - TWIN PERPENDICULARS



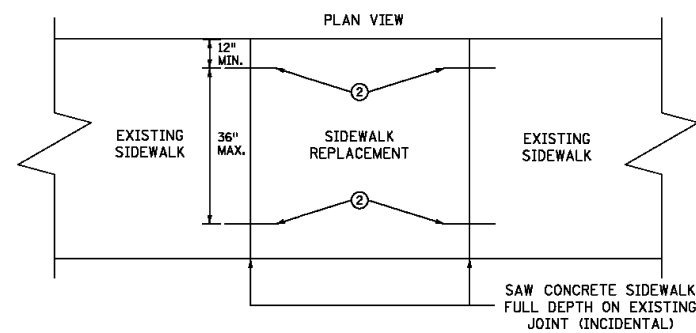
FLOW LINE PROFILE "TABLE" - FAN



FLOW LINE PROFILE RAISE - TWIN PERPENDICULARS

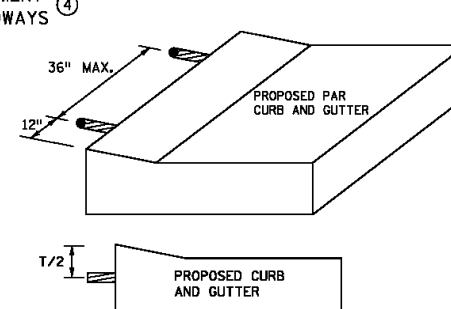


FLOW LINE PROFILE RAISE - FAN

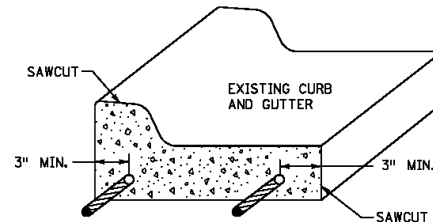


OPTIONAL SIDEWALK REINFORCEMENT

SIDEWALK REINFORCEMENT TO BE USED ONLY WHEN SPECIFIED IN THE PLAN.

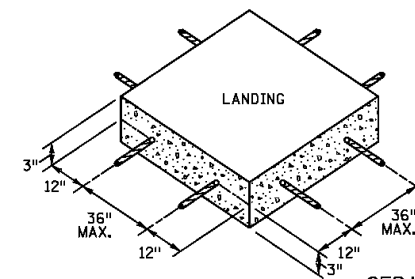


OPTIONAL CURB LINE REINFORCEMENT DETAILS ② ④

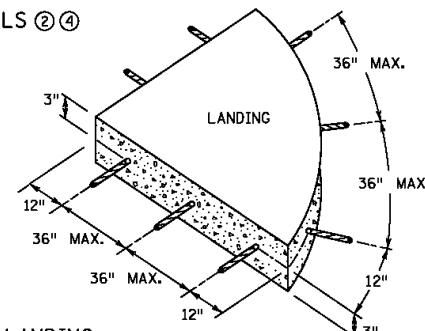


FOR USE ON CURB RAMP RETROFITS

CURB AND GUTTER REINFORCEMENT ③



SEPARATE LANDING POUR REINFORCEMENT ①



NOTES:

- ① TO ENSURE RAMPS AND LANDINGS ARE PROPERLY CONSTRUCTED, ALL INITIAL LANDINGS AT A TOP OF A RAMPED SURFACE (RUNNING SLOPE GREATER THAN 2%) SHALL BE FORMED AND PLACED SEPARATELY IN AN INDEPENDENT CONCRETE POUR. FOLLOW SIDEWALK REINFORCEMENT DETAILS ON THIS SHEET FOR ALL SEPARATELY POURED INITIAL LANDINGS.
- ② DRILL AND GROUT NO. 4 12" LONG REINFORCEMENT BARS AT 36" MAXIMUM CENTER TO CENTER (EPOXY COATED). BARS TO BE ADJUSTED TO MATCH RAMP GRADE.
- ③ DRILL AND GROUT 2 - NO. 4 X 12" LONG REINFORCEMENT BARS (EPOXY COATED). REINFORCEMENT REQUIRED FOR ALL CONSTRUCTION JOINTS WITHIN RADIUS.
- ④ THIS OPTIONAL CURB LINE REINFORCEMENT DETAIL SHOULD ONLY BE USED ON BITUMINOUS ROADWAYS WHEN SPECIFIED IN THE PLAN.
- ⑤ 1/2 IN. PREFORMED JOINT FILLER MATERIAL PER MNDOT SPEC. 3702.

REVISION:
APPROVED: JANUARY 23, 2017
OPERATIONS ENGINEER

MINNESOTA DEPARTMENT OF TRANSPORTATION

REVISOR:

APPROVED: 1-23-2017

STATE DESIGN ENGINEER

PEDESTRIAN CURB RAMP DETAILS
STANDARD PLAN 5-297.250
6 OF 6

NO. DATE BY
REVISIONS
REMARKS

PEDESTRIAN CURB RAMP DETAILS

CITY OF EDINA

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Ph: 952-826-0371

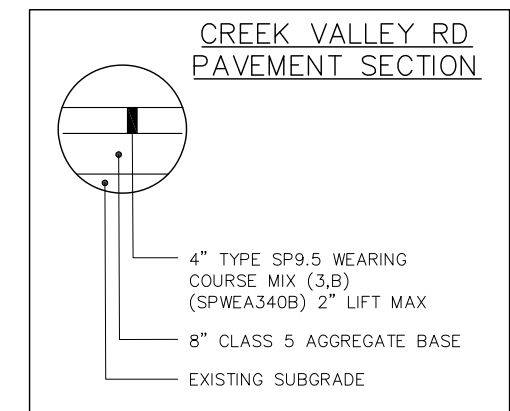
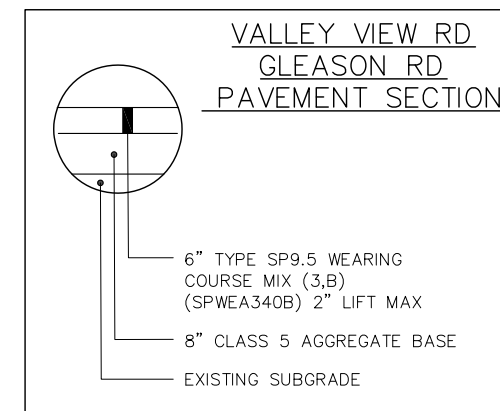
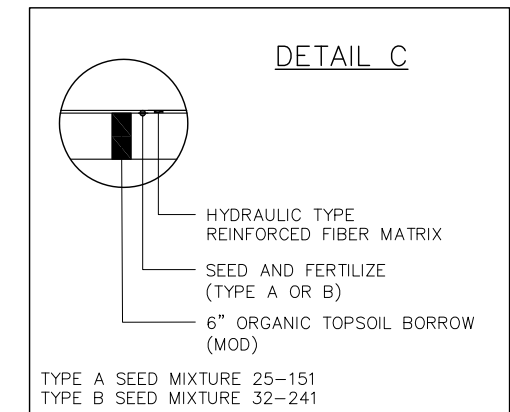
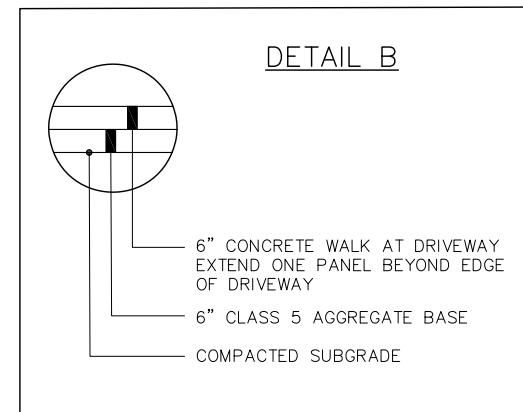
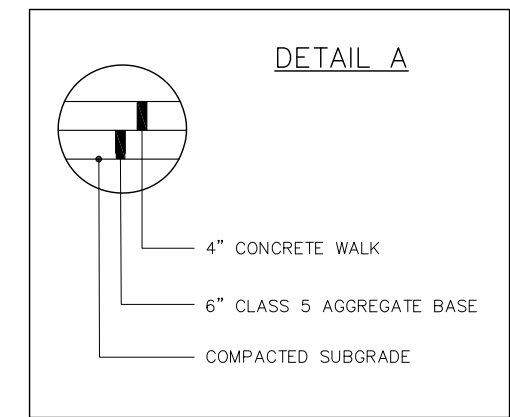
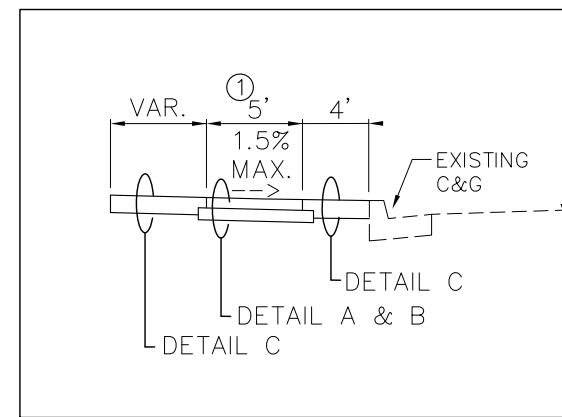
MINNESOTA CITY OF EDINA

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DATE: 06/22/20
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SHEET 8 OF 32



① PEDESTRIAN CURB RAMPS WILL BE CONSTRUCTED IN ACCORDANCE WITH MnDOT STANDARD PLANS

VALLEY VIEW RD TYPICAL SECTIONS

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7450 METRO BOULEVARD
EDINA, MN 55439-3037
Ph: 952-826-0371

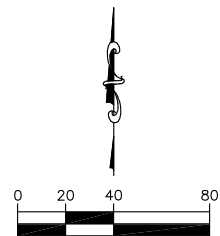


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Carol E. D'Amico

DRAWN: CJ
 CHECKED: CA
 APPROVED: AT
 DATE: 06/22/2011
 CONTR.# ENG 20-1
 SHEET 9 OF 32



LEGEND

INLET PROTECTION

→

DRAINAGE DIRECTION

INLET PROTECTION SHALL BE USED IN ALL CBs, MHs, AND Dis FOR THE DURATION OF THE PROJECT IN ACCORDANCE WITH MnDOT SPEC. 2573.

INLET PROTECTION DEVICES MAY NEED TO BE PLACED MULTIPLE TIMES IN THE SAME LOCATION THROUGHOUT THE DURATION OF THE PROJECT. CONTRACTOR MAY NEED TO USE SPECIFIC DEVICES FOR APPROPRIATE PHASES OF CONSTRUCTION. THESE CHANGES ARE CONSIDERED INCIDENTAL.

PROTECT ALL DOWNSTREAM INLET STRUCTURES OUTSIDE PROJECT LIMITS AS DIRECTED BY THE ENGINEER IN THE FIELD.

LOCATIONS OF CONCRETE WASHOUT SITES SHALL BE DETERMINED IN THE FIELD BY THE CONTRACTOR AND THE ENGINEER.

ALL AREAS DISTURBED BY CONSTRUCTION SHALL RECEIVE 6” OF TOPSOIL AND BE SEEDED WITHIN 14 DAYS OF COMPLETING THE WORK NECESSARY TO PROCEED WITH FINAL GRADING. TOPSOIL AND SEEDING TO BE COMPLETED BY OTHERS.

PARCEL SIZE = N/A

DISTURBED AREA = 8,800 SQFT

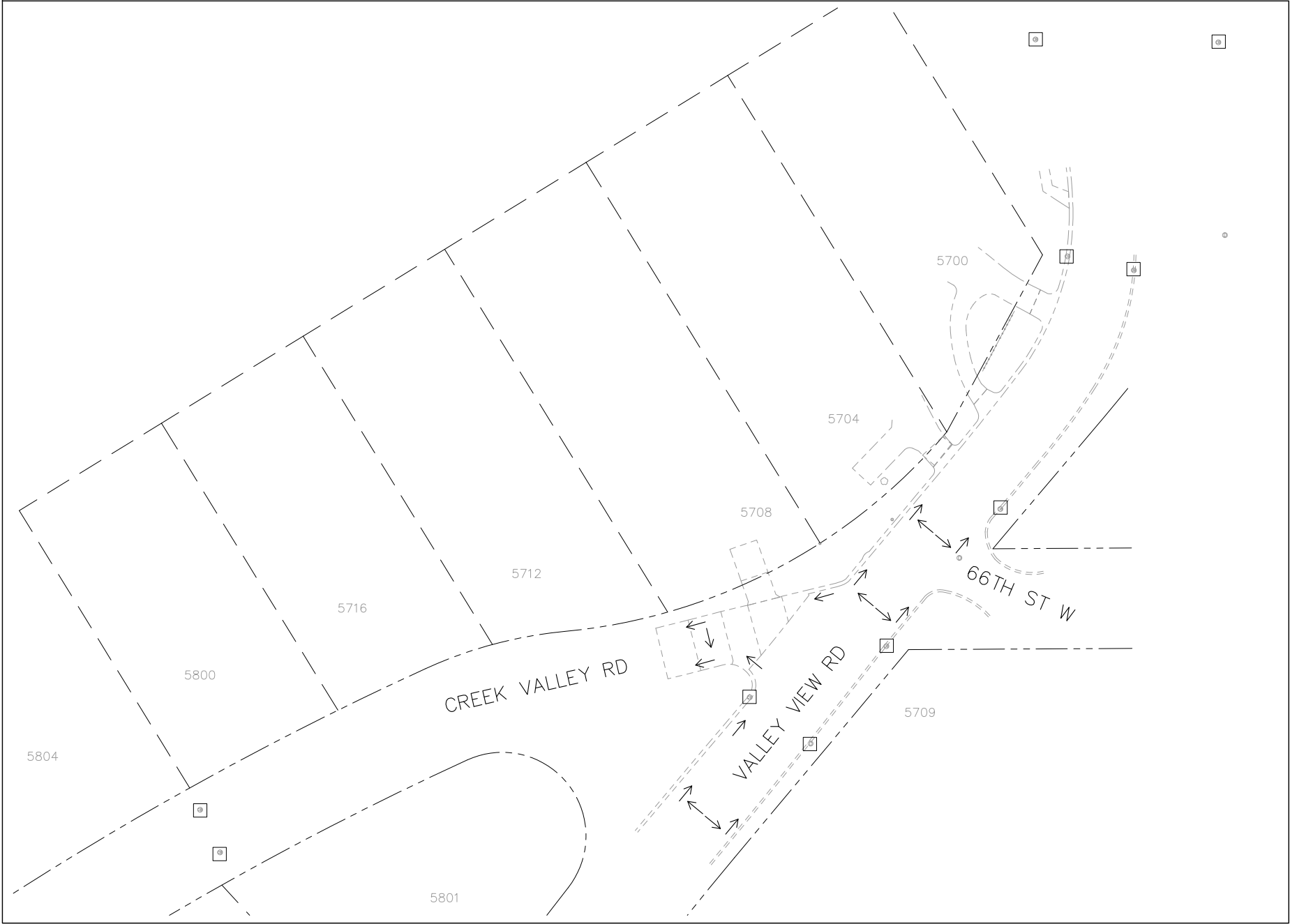
EXISTING IMPERVIOUS = 4,682 SQFT

LENGTH OF SHORELINE AFFECTED = 0 FT

VOLUME OF EXCAVATION/FILL = 120 CUYD

PROPOSED IMPERVIOUS = 5,266 SQFT

WATERBODY = NINE MILE CREEK



AGENCY CONTACTS:


NINE MILE CREEK WATERSHED DISTRICT
12800 GERARD DRIVE
EDEN PRAIRIE, MN 55346
PHONE: 952-835-2078

MINNESOTA POLLUTION CONTROL AGENCY
520 LAFAYETTE ROAD
ST. PAUL, MN 55155-4194
PHONE: 651-296-6300
MPCA DUTY OFFICER: 651-649-5451

NO.		DATE	BY	REMARKS

VALLEY VIEW ROAD
STORMWATER
POLLUTION
PREVENTION PLAN

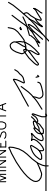
CITY OF EDINA



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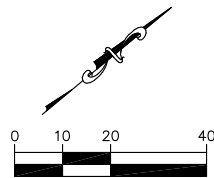
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DATE: 06/22/20

CONTR.#/ENG 20-13

SHEET 10 OF 32



- Ⓒ CONNECT TO EXISTING STORM SEWER

Ⓓ LANDSCAPE MATERIAL SPECIAL

Ⓜ SALVAGE AND INSTALL MAILBOX AND SUPPORT

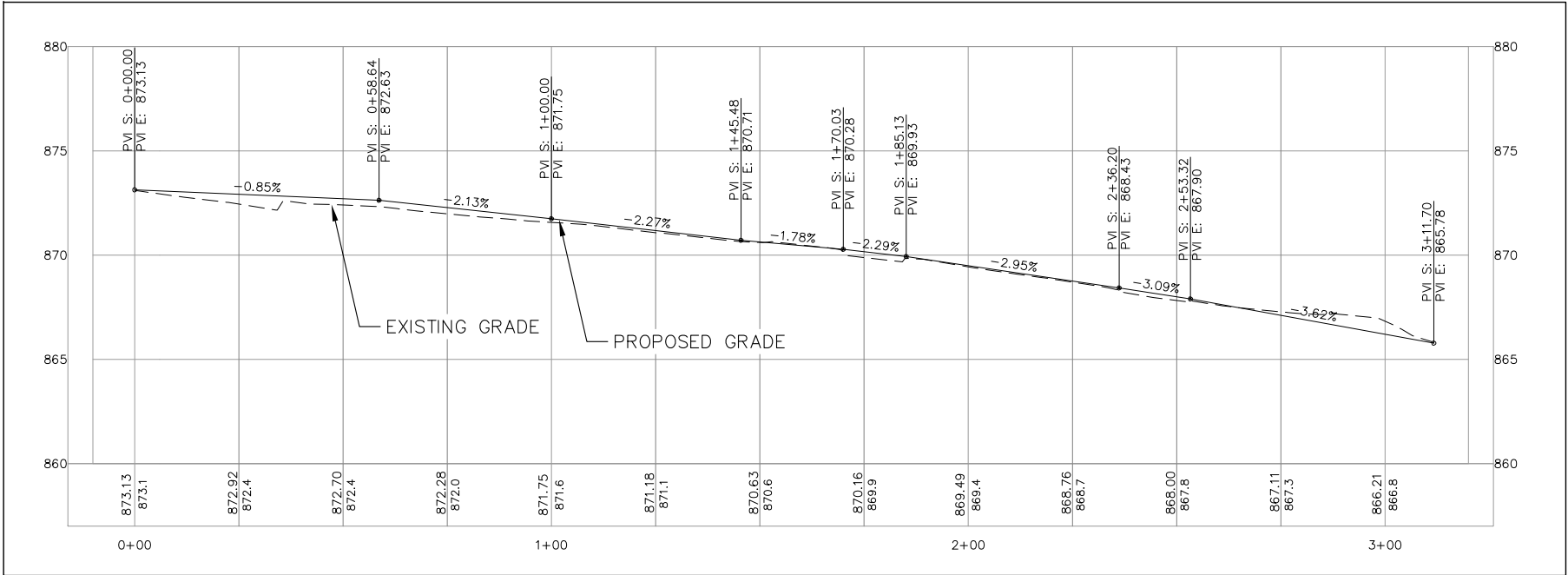
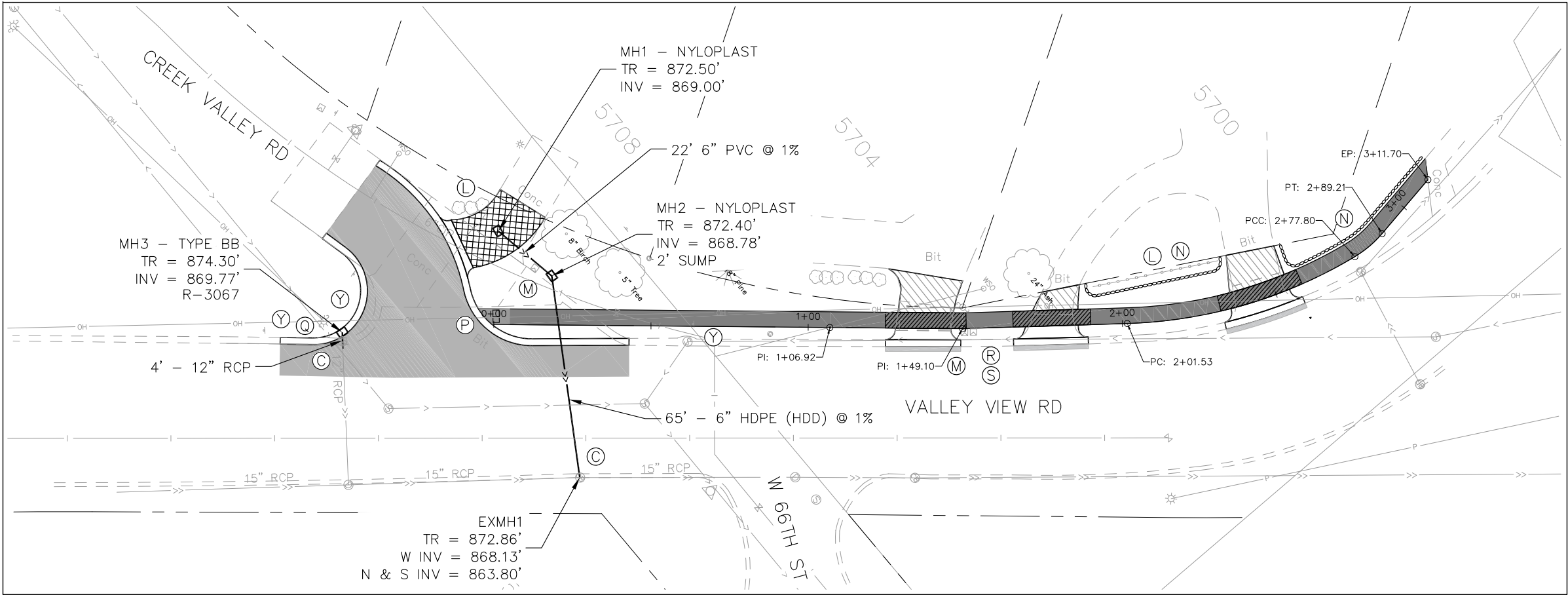
Ⓝ NATURAL STONE RETAINING WALL

Ⓟ PEDESTRIAN CURB RAMP WITH TRUNCATED DOMES

Ⓠ HOLD UTILITY POLE (BY OTHERS)
- Ⓡ REMOVE MAIL BOX AND SUPPORT

Ⓢ FURNISH AND INSTALL MAIL BOX AND SUPPORT

Ⓨ SALVAGE AND INSTALL SIGN



VALLEY VIEW ROAD
CONSTRUCTION PLAN
AND PROFILE

CITY OF EDINA

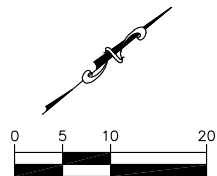
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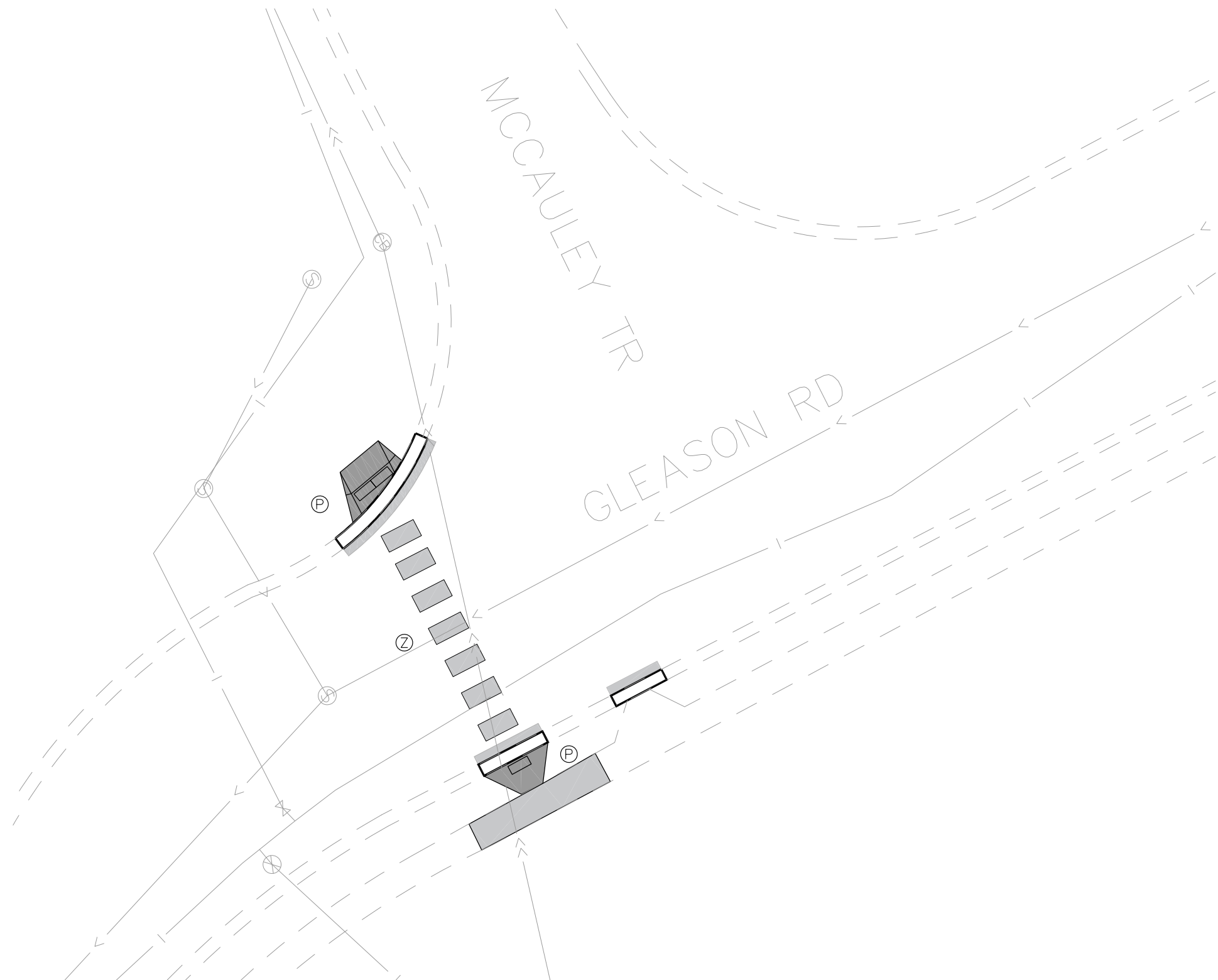
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OF MINNESOTA

Caren E. Rapp
DATE: 06/22/20 LIC. NO. 42003

DRAWN: C/JG
CHECKED: CAM
APPROVED: ATD
DATE: 06/22/20
CONTR.#/ENG 20-13
SHEET 11 OF 32



- ⑦ PEDESTRIAN CURB RAMP WITH TRUNCATED DOMES
- ⑧ CROSSWALK MARKING – THERMOPLASTIC (GROUND IN)



2020 PEDESTRIAN AND BICYCLE IMPROVEMENTS



CITY OF EDINA
7450 METRO BOULEVARD
EDINA, MN 55439-3037
Ph: 952-826-0371

GLEASON RD AND
MCCAULEY TR
CONSTRUCTION PLAN

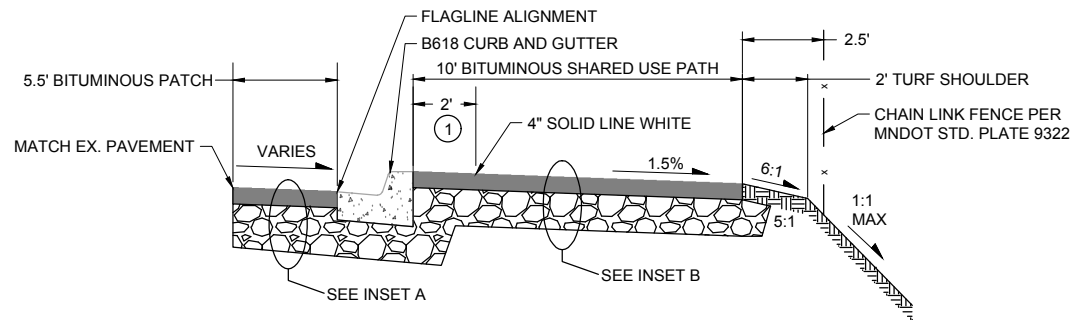
I HEREBY CERTIFY THAT THIS
PLAN WAS PREPARED BY ME OR
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AND THAT I AM A DULY
LICENSED PROFESSIONAL ENGINEER
UNDER THE LAWS OF THE STATE
OF MINNESOTA

DATE: 06/22/20 LIC. NO. 42003

DRAWN:	CJG
CHECKED:	CAM
APPROVED:	ATD
DATE:	06/22/20
CONTR.#	ENG 20-13
SHEET	12 OF 32

REMARKS
REVISIONS

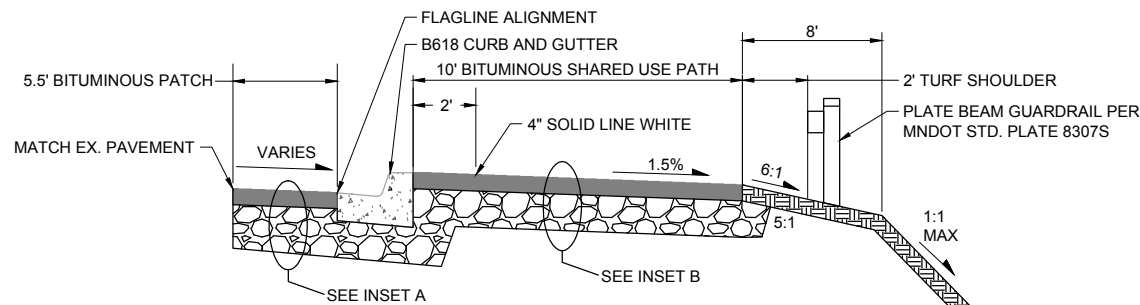
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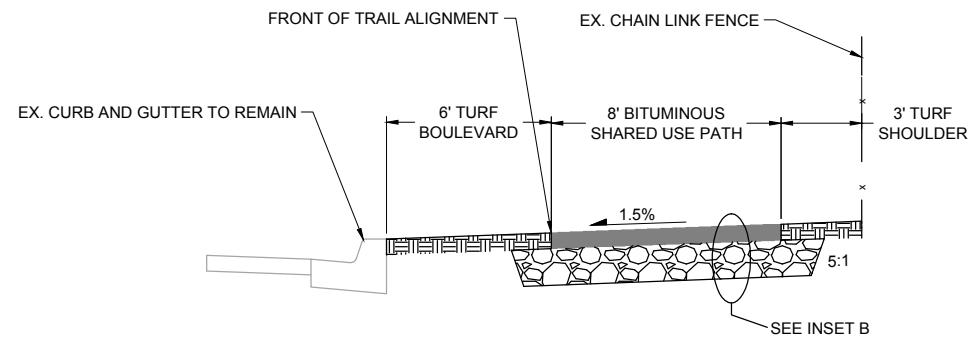
KEYED NOTES ○

1. WIDTH IS 7' FROM STA. 0+00 TO STA. 1+43.63 TRANSITION FROM 7' TO 10' FROM STA. 1+43.63 TO STA. 1+91.86.

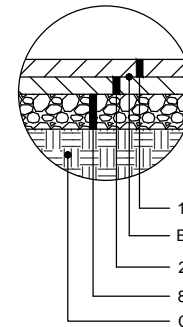
TYPICAL SECTION - SOUTH OF TH-62
TRACY AVE SHARED USE PATH
STA. 0+00 TO STA. 6+25



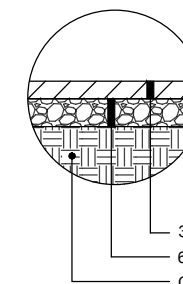
TYPICAL SECTION - SOUTH OF TH-62
TRACY AVE SHARED USE PATH
STA. 6+25 TO STA. 7+41.66



TYPICAL SECTION - NORTH OF TH-62
TRACY AVE SHARED USE PATH
STA. 0+00 TO STA. 2+18.96



INSET A



INSET B

DRAWN BY:	SD_SS
DESIGNER:	KM
CHECKED BY:	WB
DESIGN TEAM	

NO.	BY	DATE

REVISIONS

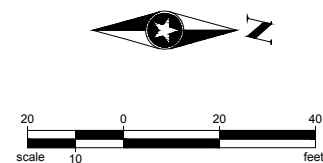
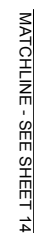
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SUITE 300
MINNETONKA, MN 55343
www.sehinc.com

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William J. Bauer
Date: 06/22/2020
WILLIAM J. BAUER, PE
Lic. No. 55680

**EDINA,
MINNESOTA**

TYPICAL SECTIONS

FILE NO.
ED150253



TRACY AVE.

OFF-RAMP TH 62 EB

LEGEND

REMOVE CURB AND GUTTER

REMOVE CASTING

REMOVE STORM SEWER PIPE

SAWCUT (FULL DEPTH)

REMOVE PAVEMENT LANE LINE

REMOVE PAVEMENT MARKING SYMBOL

REMOVE PLATE BEAM GUARDRAIL

REMOVE BITUMINOUS PAVEMENT

REMOVE CONCRETE PAVEMENT

REMOVE CONCRETE SIDEWALK

CLEAR AND GRUB

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DESIGNER: KM
CHECKED BY: WB
DESIGN TEAM

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William J. Bauer WILLIAM J. BAUER, PE

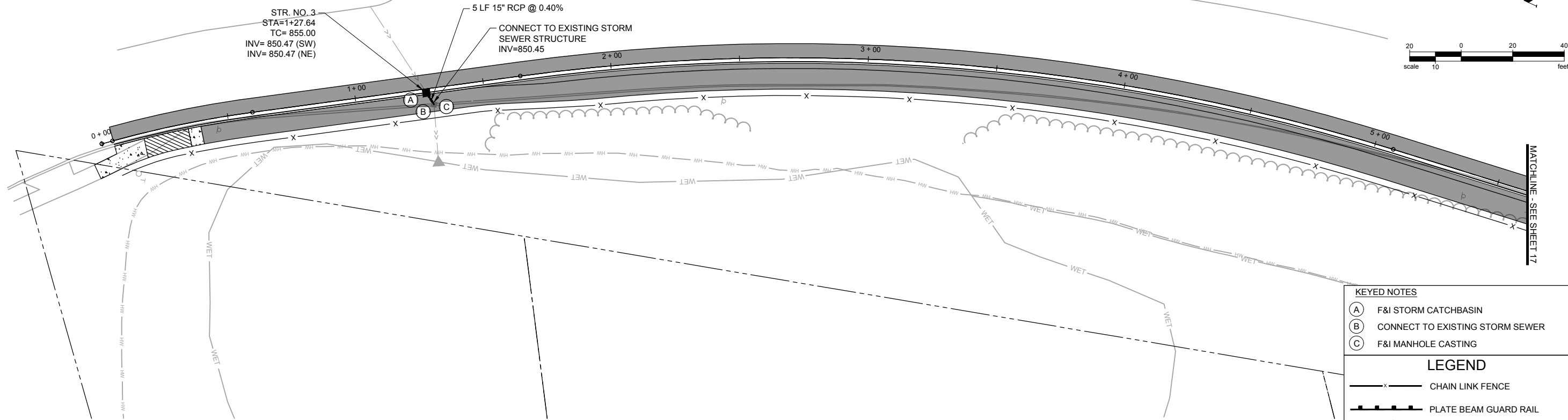
Date: 06/22/2020 Lic. No. 55680

EDINA,
MINNESOTA

**INPLACE TOPO AND REMOVALS
- TRACY AVE**

FILE NO.
ED150253

15	32
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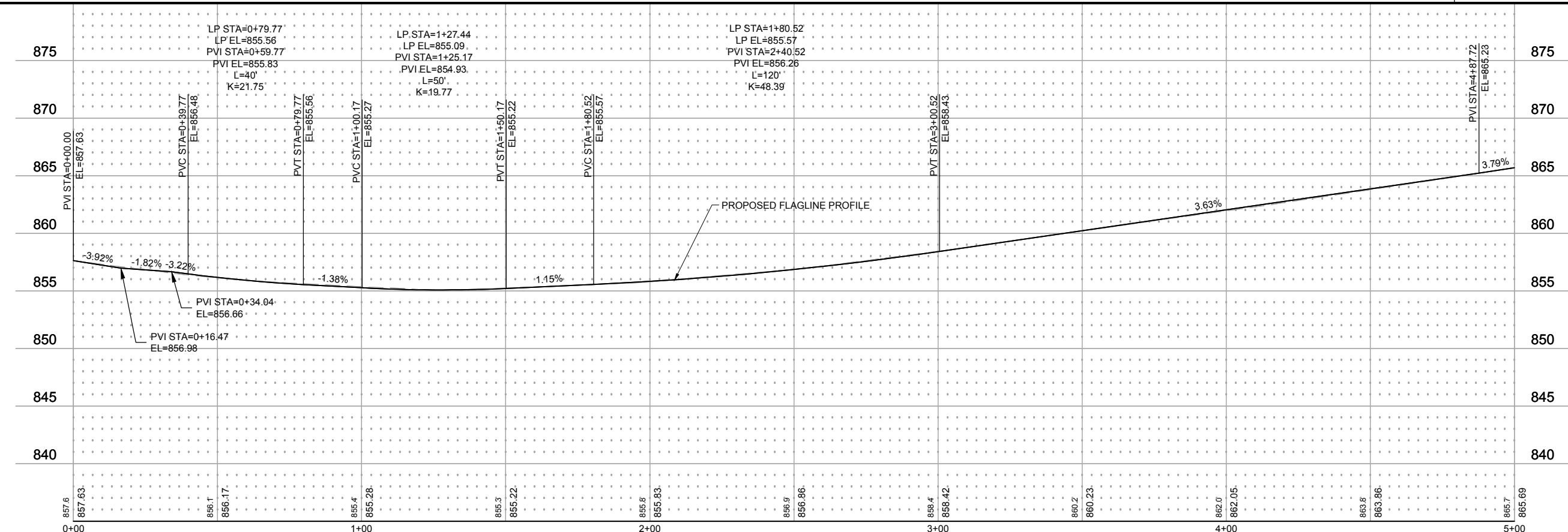
KEYED NOTES

- (A) F&I STORM CATCHBASIN
- (B) CONNECT TO EXISTING STORM SEWER
- (C) F&I MANHOLE CASTING


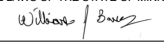
LEGEND

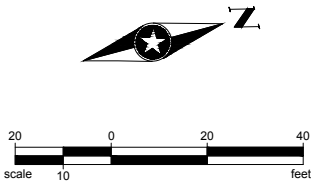
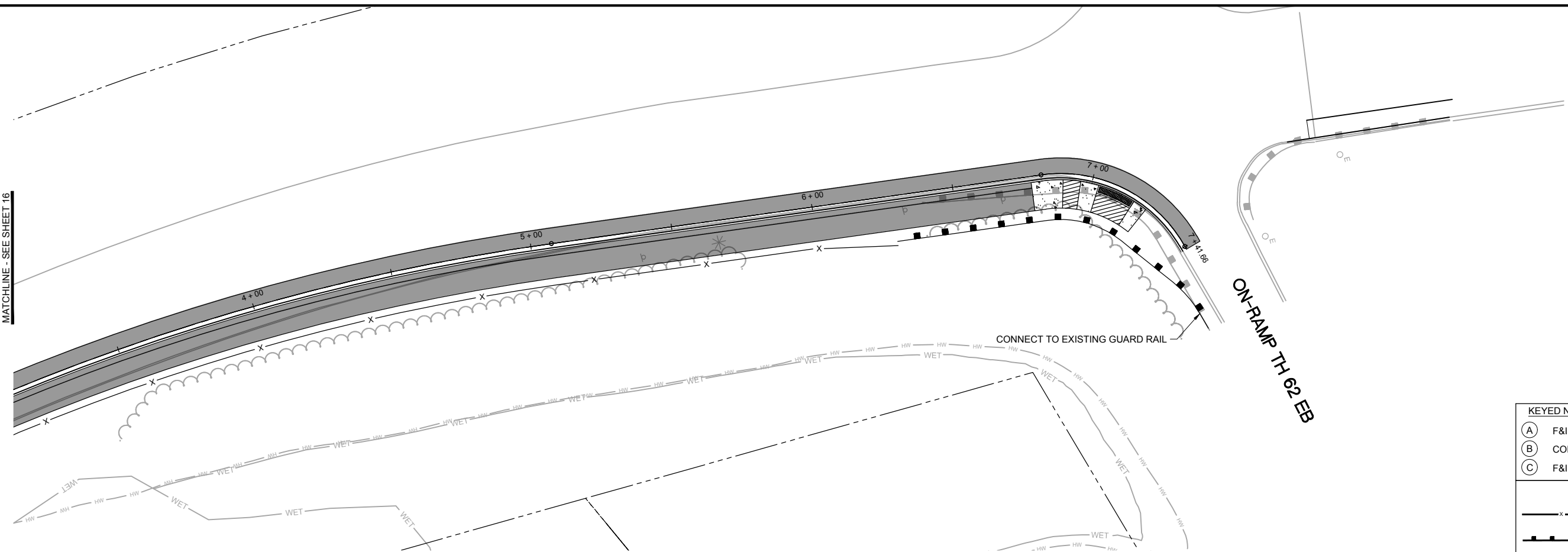
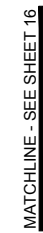
- CHAIN LINK FENCE
- PLATE BEAM GUARD RAIL
- CONCRETE SIDEWALK
- BITUMINOUS PAVEMENT/TRAIL

TRACY AVENUE



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DRAWN BY: SD, SS	<table><tr><td>NO.</td><td>BY</td><td>DATE</td><td>REVISIONS</td></tr><tr><td> </td><td> </td><td> </td><td> </td></tr><tr><td> </td><td> </td><td> </td><td> </td></tr><tr><td> </td><td> </td><td> </td><td> </td></tr></table>	NO.	BY	DATE	REVISIONS													 <p>PHONE: 952.912.2600 10901 RED CIRCLE DRIVE, SUITE 300 MINNETONKA, MN 55343 www.sehinc.com</p>	I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.  Date: 06/22/2020 Lic. No. 55680	EDINA, MINNESOTA	PLAN AND PROFILE - SOUTH OF TH 62	FILE NO. ED150253	16 32
NO.		BY	DATE	REVISIONS																			
DESIGNER: KM																							
CHECKED BY: WB																							
DESIGN TEAM																							



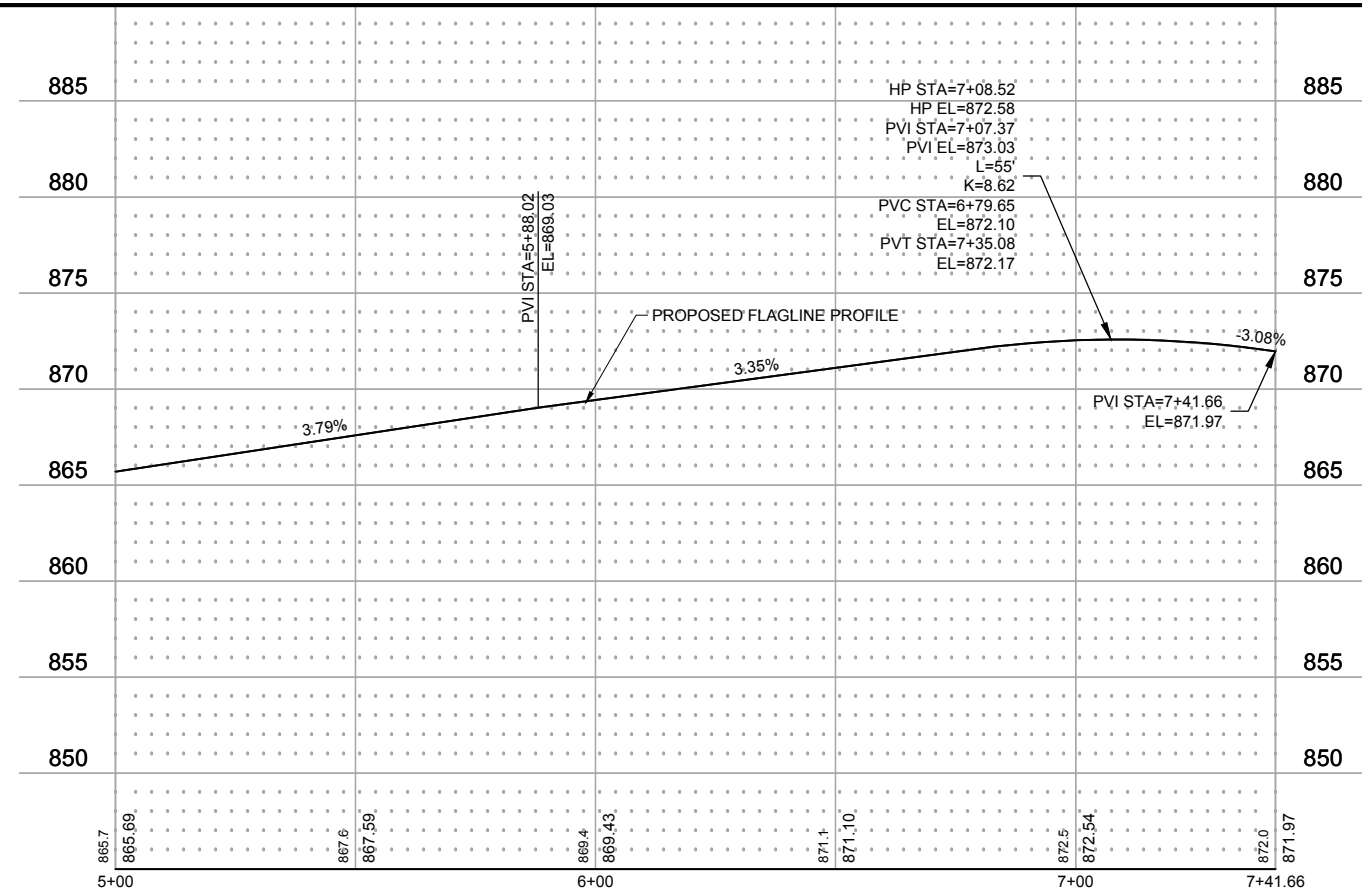
KEYED NOTES

- (A) F&I STORM CATCHBASIN
- (B) CONNECT TO EXISTING STORM SEWER
- (C) F&I MANHOLE CASTING

LEGEND

- | | |
|---|---------------------------|
|  | CHAIN LINK FENCE |
|  | PLATE BEAM GUARD RAIL |
|  | CONCRETE SIDEWALK |
|  | BITUMINOUS PAVEMENT/TRAIL |

TRACY AVENUE



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William J. Bauer WILLIAM J. BAUER, PE

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EDINA,
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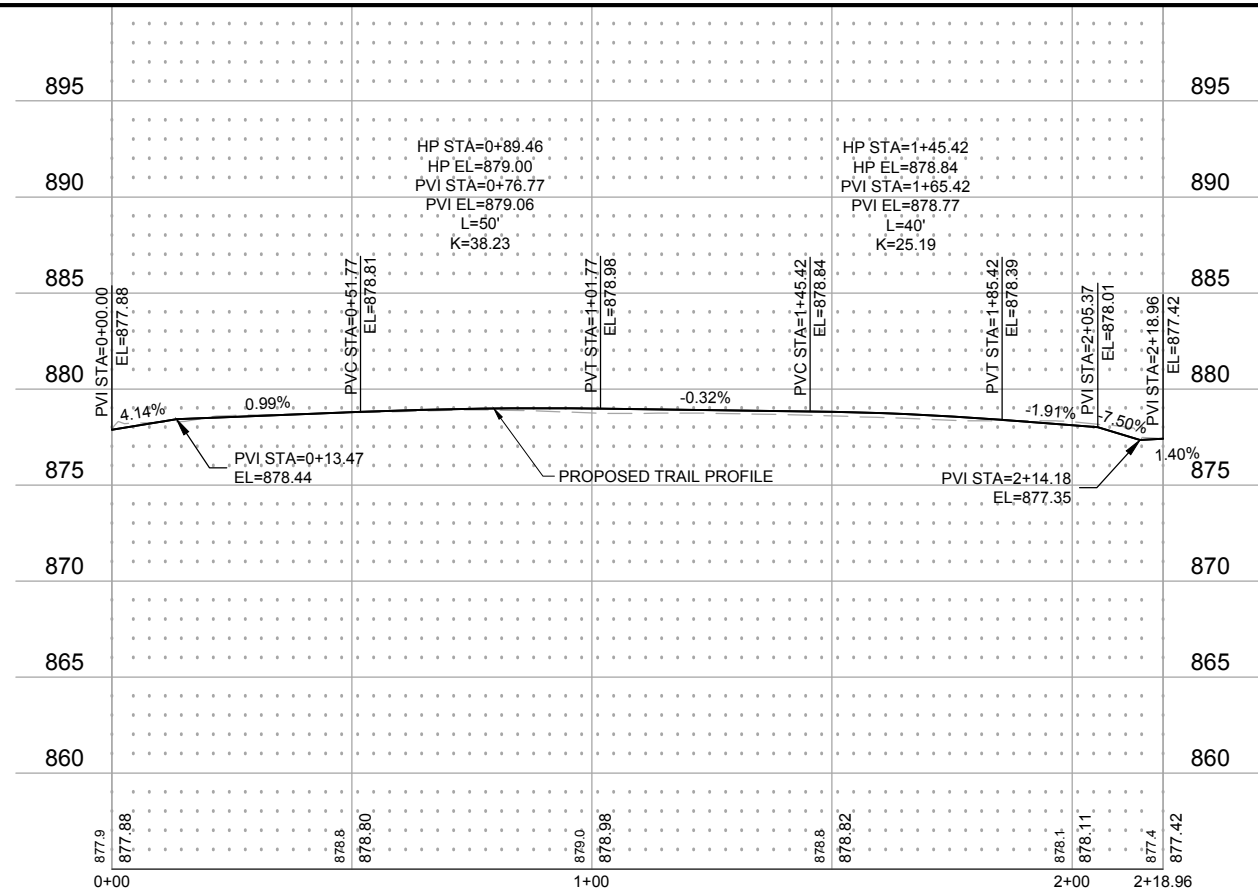
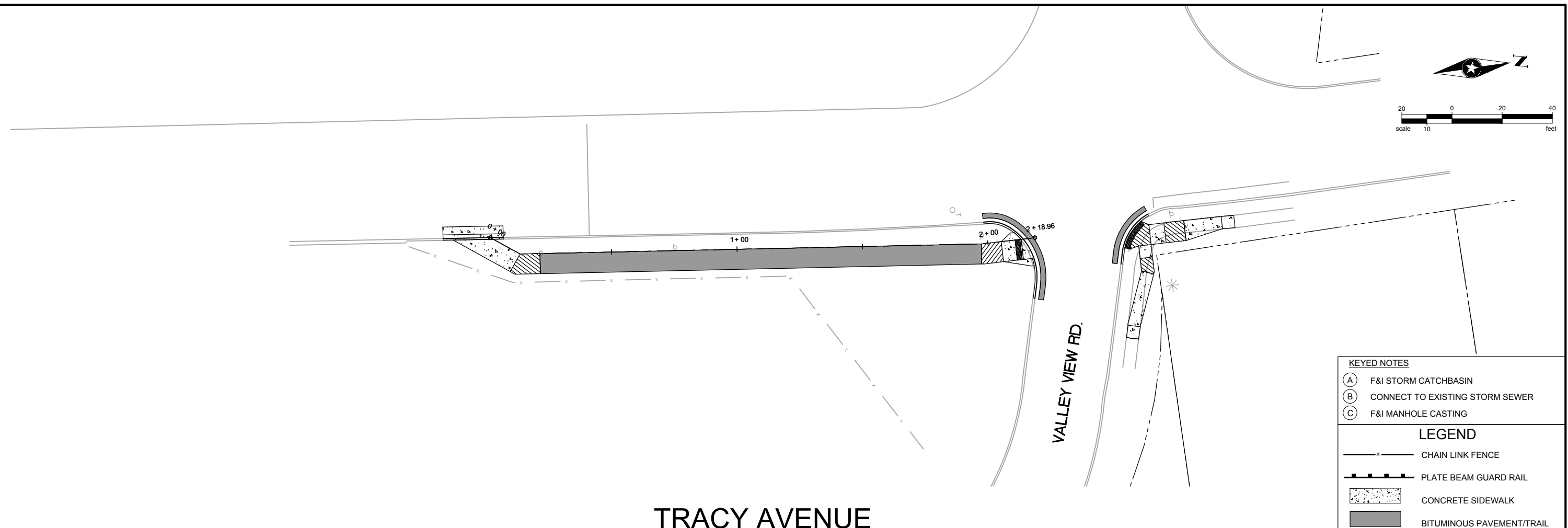
PLAN AND PROFILE - SOUTH OF TH 62

FILE NO.
ED150253

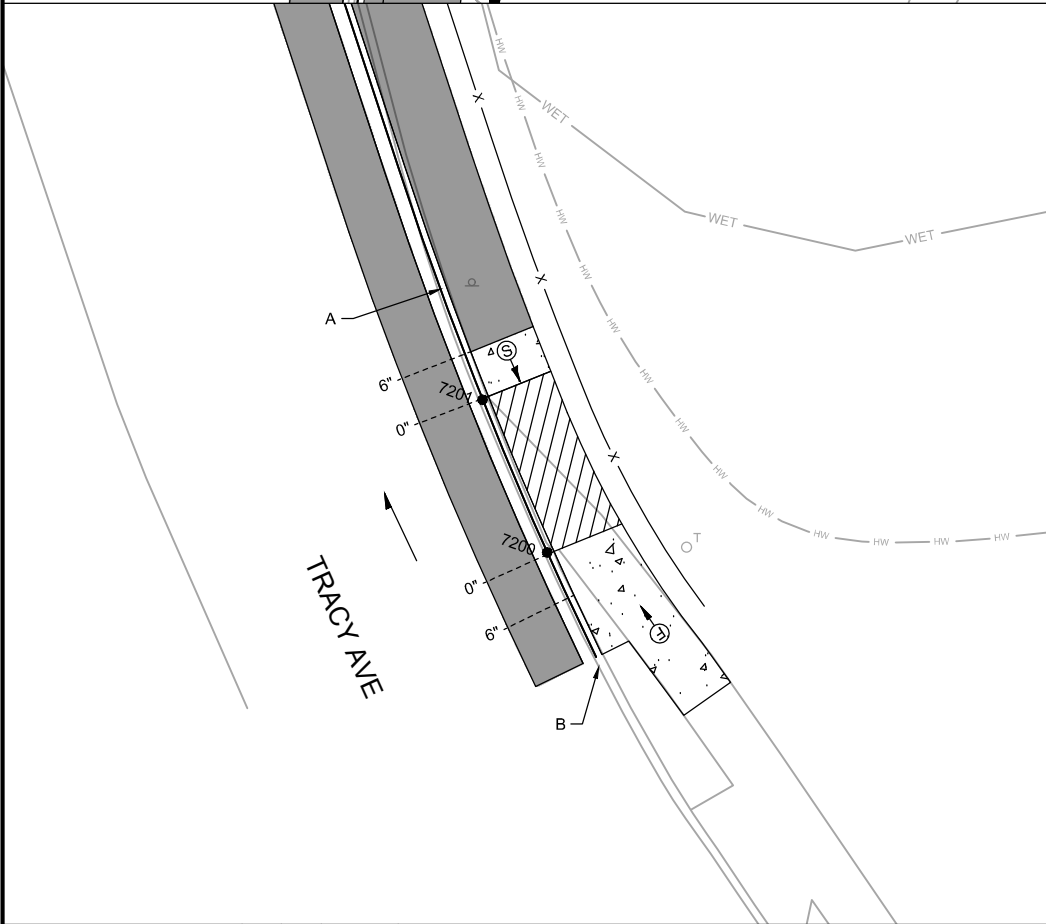
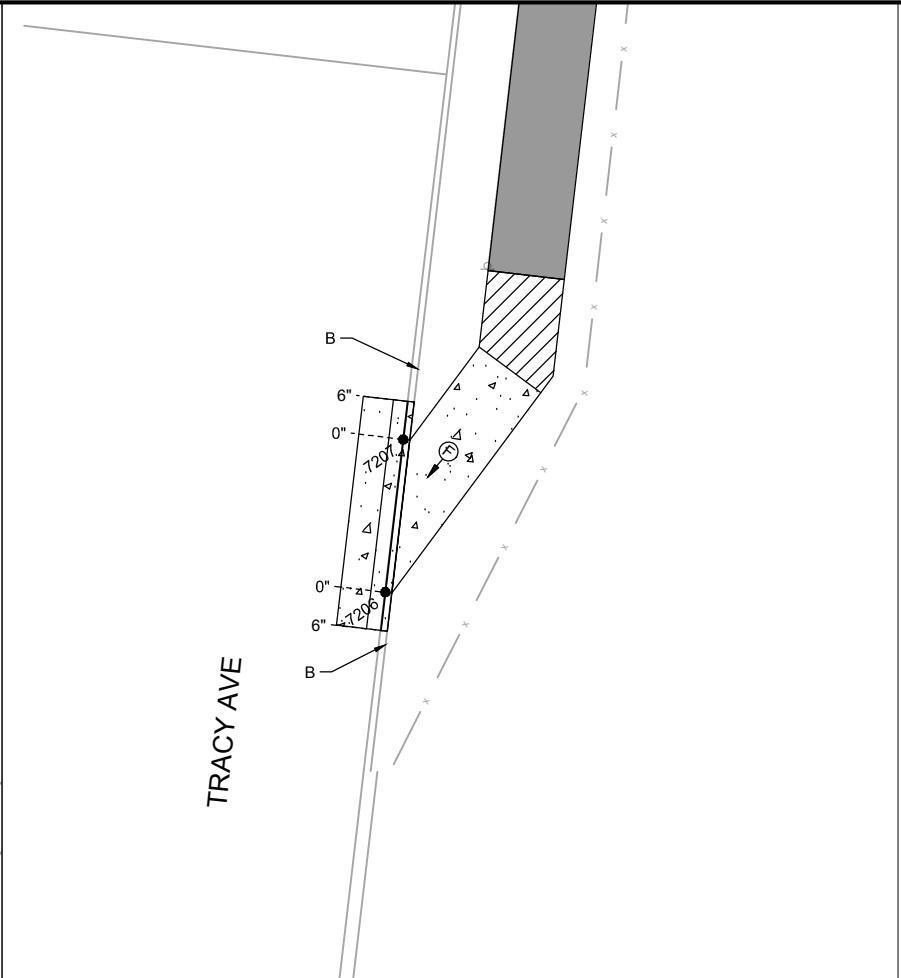
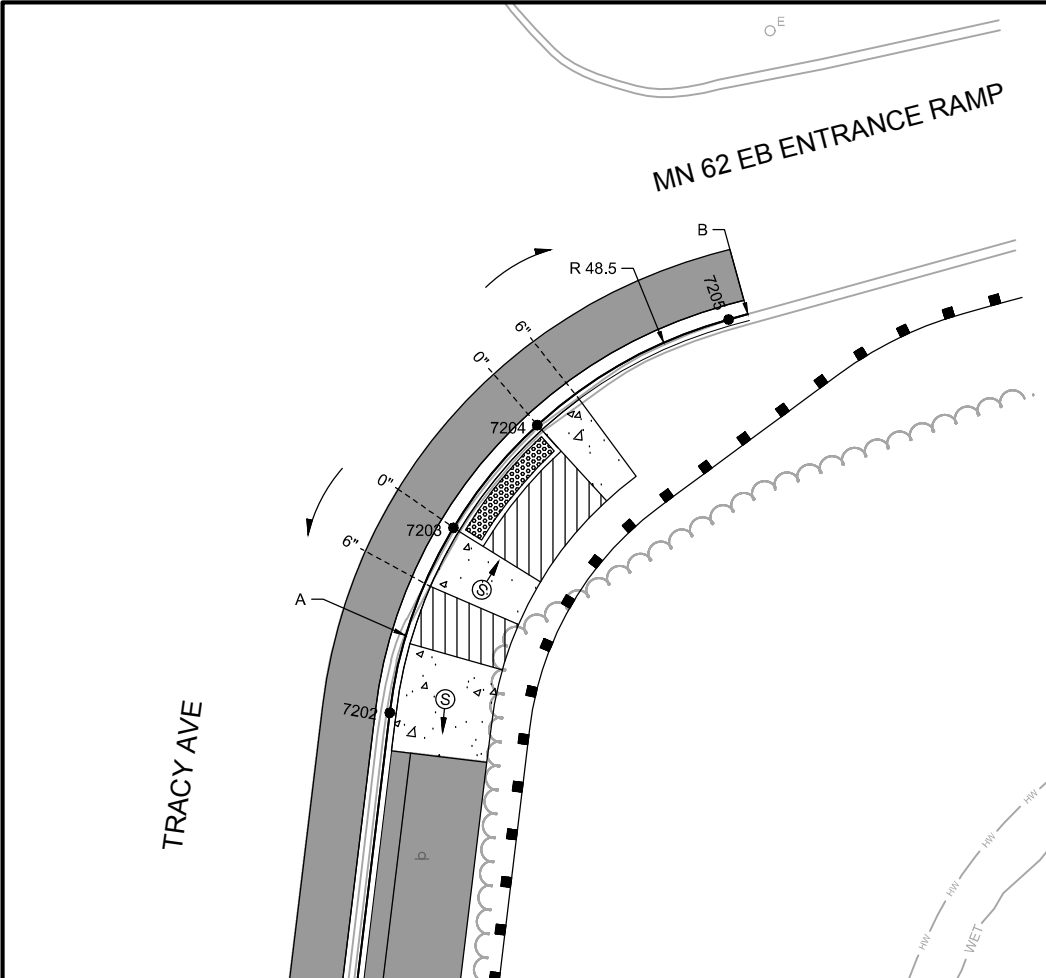
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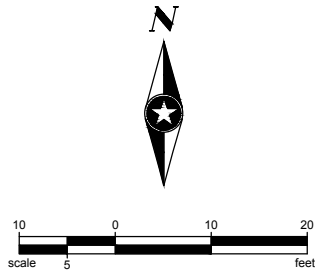
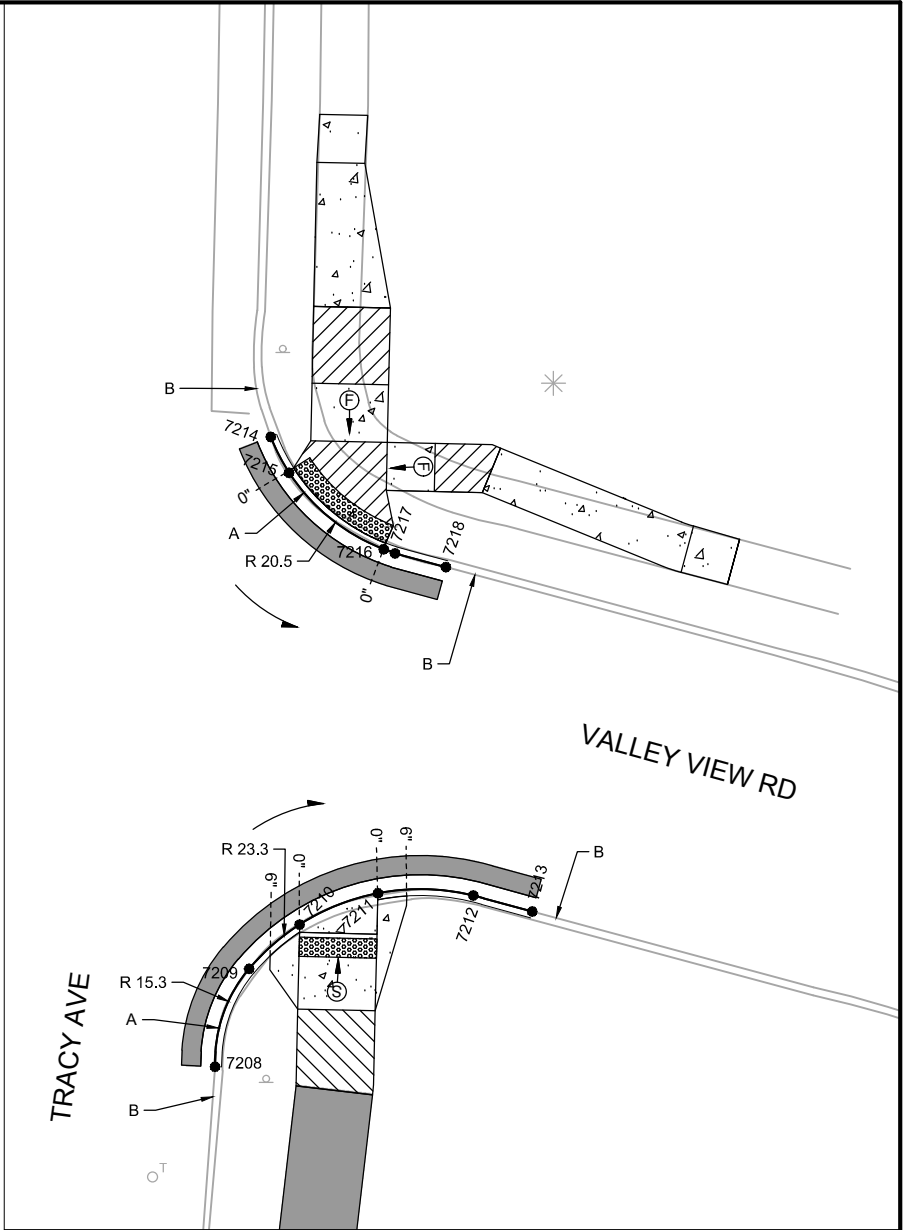
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POINT TABLE				
POINT #	RAW DESCRIPTION	ELEVATION	NORTHING	EASTING
7200	PED RAMP - 0"	856.86	134215.1489	503772.3625
7201	PED RAMP - 0"	856.57	134231.0442	503765.6381
7202	FLOWLINE (ER 48.5)	872.07	134865.4672	503711.7362
7203	PED RAMP - 0"	872.47	134884.7173	503718.3691
7204	PED RAMP - 0"	872.45	134895.3974	503727.1138
7205	FLOWLINE (ER 48.5)	871.91	134906.4113	503747.0357
7206	PED RAMP - 0"	877.74	135242.4130	503754.9035
7207	PED RAMP - 0"	877.89	135258.3221	503756.7795
7208	FLOWLINE (ER 15.3)	878.01	135452.6429	503777.4237
7209	FLOWLINE (ER 15.3/23.3)	877.61	135462.8376	503780.9859
7210	PED RAMP - 0"	877.35	135467.4550	503786.2478
7211	PED RAMP - 0"	877.17	135470.7115	503794.3893
7212	FLOWLINE (ER 23.3)	876.45	135470.4796	503804.3259
7213	FLOWLINE	875.99	135468.8052	503810.4743
7214	FLOWLINE (ER 20.5)	877.17	135518.2482	503783.2202
7215	PED RAMP - 0"	876.99	135514.5061	503785.1372
7216	PED RAMP - 0"	876.76	135506.5320	503794.9977
7217	FLOWLINE (ER 20.5)	876.71	135506.1228	503796.1728
7218	FLOWLINE	876.50	135504.6902	503801.4727



KEYED NOTES:
A. B618 CURB AND GUTTER
B. PROTECT EXIST. CURB AND GUTTER

LAND AREA- 4'X4' MIN. DIMS. MAX 2.0% SLOPE IN ALL DIRECTIONS

TRUNCATED DOMES, SEE STANDARD PLATE 7038

6" CONCRETE SIDEWALK RAMP

INDICATES PEDESTRIAN RAMP- SLOPE SHALL BE BETWEEN 5.0% MINIMUM AND 8.3% MAXIMUM IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%

INDICATES PEDESTRIAN RAMP- SLOPE SHALL BE GREATER THAN 2.0% AND LESS THAN 5.0% IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%

DRAINAGE FLOW ARROW

DETAIL POINT

DRAWN BY:	SD, SS				
DESIGNER:	KM				
CHECKED BY:	WB				
DESIGN TEAM		NO.	BY	DATE	REVISIONS

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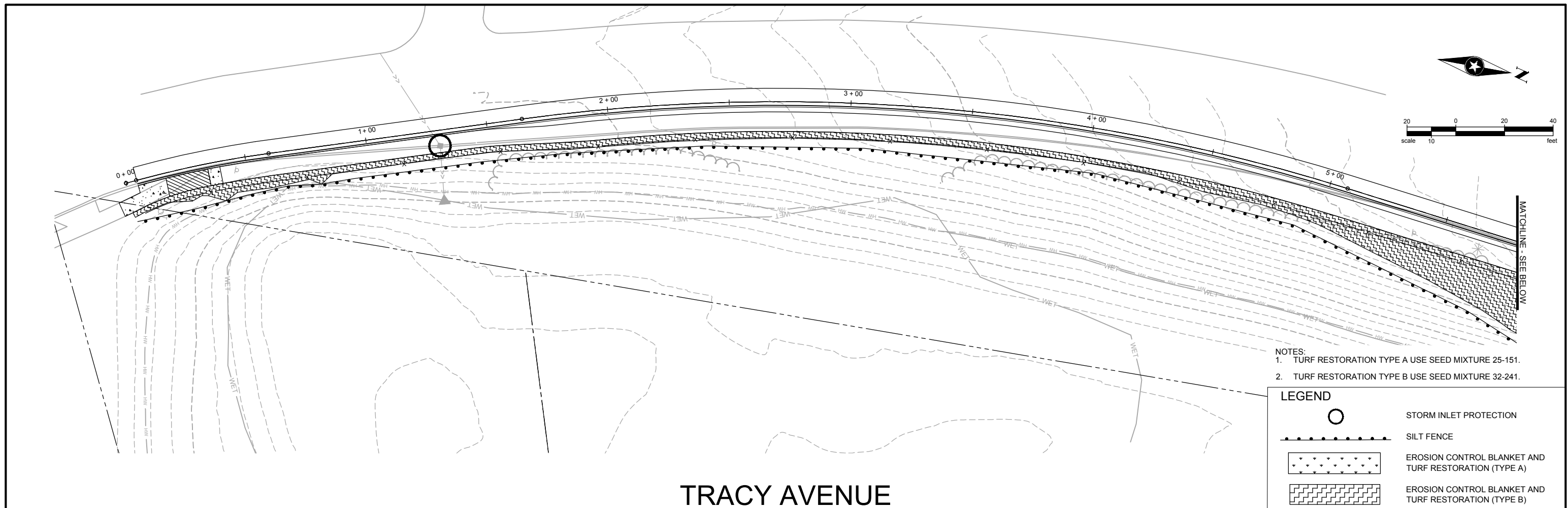
WILLIAM J. BAUER, PE
Date: 06/22/2020 Lic. No. 55680

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MINNESOTA

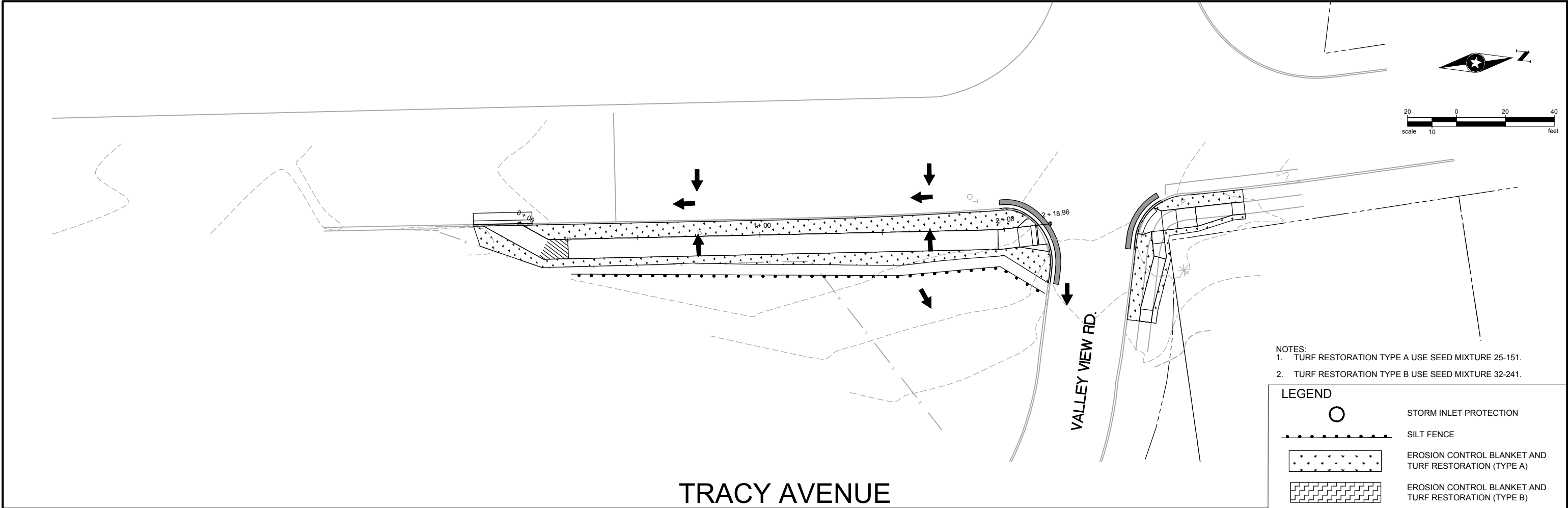
SPOT ELEVATIONS AND
INTERSECTION DETAILS

FILE NO.
ED150253

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TRACY AVENUE

- NOTES:
1. TURF RESTORATION TYPE A USE SEED MIXTURE 25-151.
 2. TURF RESTORATION TYPE B USE SEED MIXTURE 32-241.

LEGEND

- | | |
|--|---|
| | STORM INLET PROTECTION |
| | SILT FENCE |
| | EROSION CONTROL BLANKET AND TURF RESTORATION (TYPE A) |
| | EROSION CONTROL BLANKET AND TURF RESTORATION (TYPE B) |
| | DRAINAGE FLOW ARROW |

STORMWATER POLLUTION PREVENTION PLAN

INLET PROTECTION SHALL BE USED IN ALL CBs, MHs, AND DIS FOR THE DURATION OF THE PROJECT IN ACCORDANCE WITH MnDOT SPEC. 2573.

INLET PROTECTION DEVICES MAY NEED TO BE PLACED MULTIPLE TIMES IN THE SAME LOCATION THROUGHOUT THE DURATION OF THE PROJECT. CONTRACTOR MAY NEED TO USE SPECIFIC DEVICES FOR APPROPRIATE PHASES OF CONSTRUCTION. THESE CHANGES ARE CONSIDERED INCIDENTAL.

PROTECT ALL DOWNSTREAM INLET STRUCTURES OUTSIDE PROJECT LIMITS AS DIRECTED BY THE ENGINEER IN THE FIELD.

LOCATIONS OF CONCRETE WASHOUT SITES SHALL BE DETERMINED IN THE FIELD BY THE CONTRACTOR AND THE ENGINEER.

ALL AREAS DISTURBED BY CONSTRUCTION SHALL RECEIVE 6" OF TOPSOIL AND BE SEEDED WITHIN 14 DAYS OF COMPLETING THE WORK NECESSARY TO PROCEED WITH FINAL GRADING. TOPSOIL AND SEEDING TO BE COMPLETED BY OTHERS.

TOTAL DISTURBED AREA = 0.56 ACRES
TOTAL NEW IMPERVIOUS = 0.14 ACRES

AGENCY CONTACTS:

NINE MILE CREEK WATERSHED DISTRICT
12800 GERARD DRIVE
EDEN PRAIRIE, MN 55346
PHONE: 952-835-2078

MINNESOTA POLLUTION CONTROL AGENCY
520 LAFAYETTE ROAD
ST. PAUL, MN 55155-4194
PHONE: 651-296-6300
MPCA DUTY OFFICER: 651-649-5451

DRAWN BY: SD, SS
DESIGNER: KM
CHECKED BY: WB

DESIGN TEAM

NO.	BY	DATE

REVISIONS



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William J. Bauer

WILLIAM J. BAUER, PE
Lic. No. 55680

Date: 06/22/2020

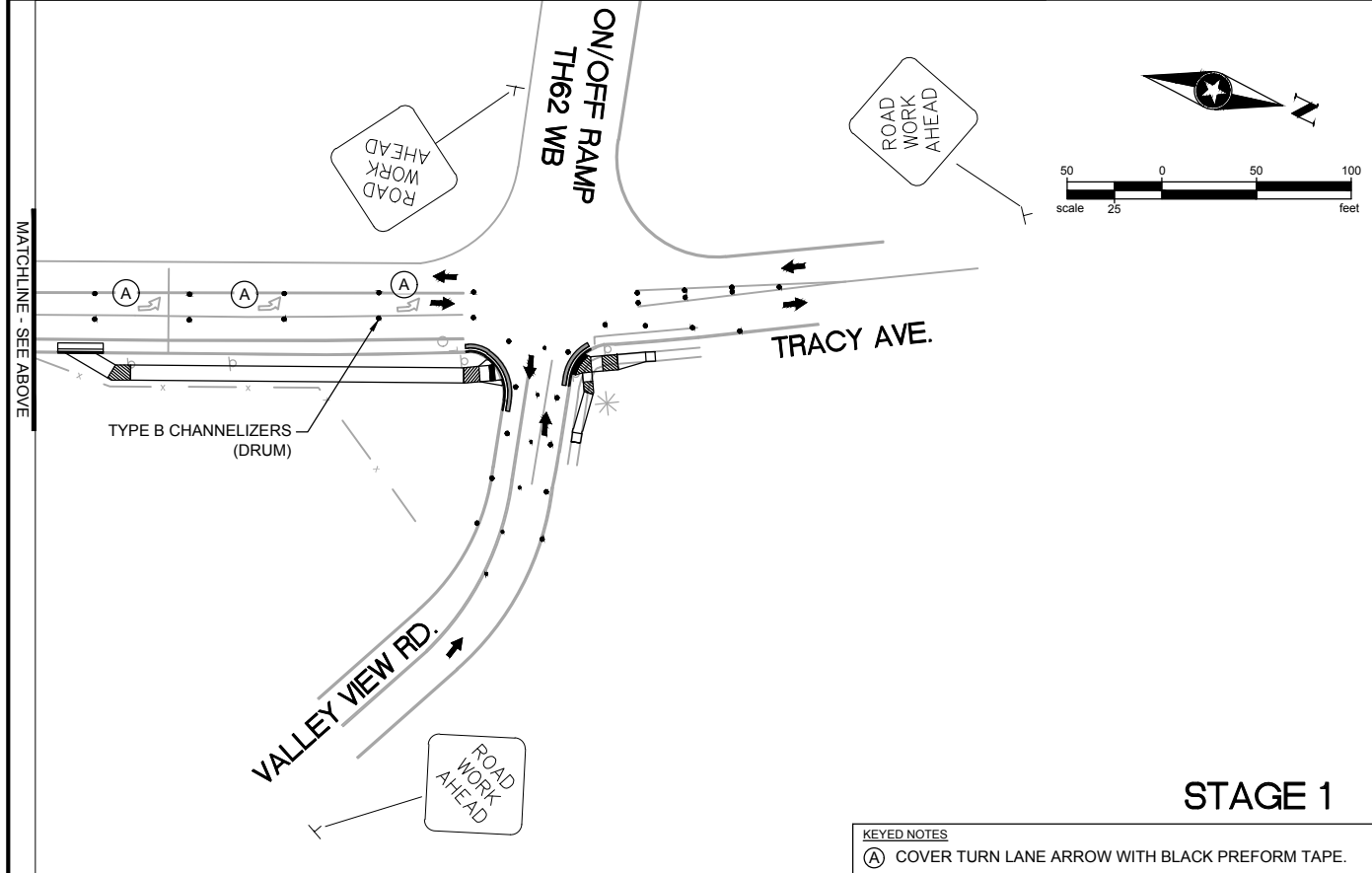
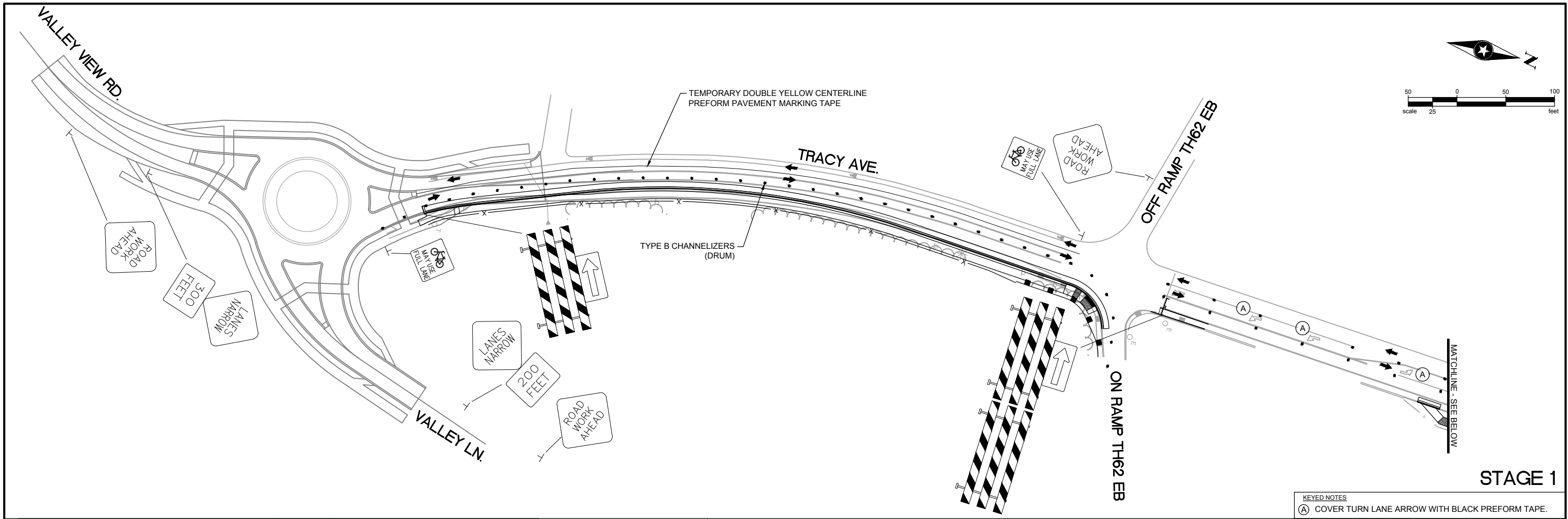
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MINNESOTA

EROSION CONTROL, TURF
ESTABLISHMENT, AND SWPPP

FILE NO.
ED150253

21

32



- GENERAL STAGING NOTES:
1. THE CONTRACTOR SHALL PROTECT ALL WORK AREAS AT ALL TIMES TO PROVIDE FOR SAFE MOVEMENT OF LOCAL TRAFFIC TO THE SATISFACTION OF THE ENGINEER.
 2. MAINTAIN MINIMUM 11' TRAVEL LANES AT ALL TIMES.

- STAGES 1A/1B NOTES:
1. PLACE TRAFFIC CONTROL SIGNAGE, CHANNELIZERS AND TEMPORARY PAVEMENT MARKINGS ALONG TRACY AVE FROM VALLEY LANE ROUNDABOUT TO VALLEY VIEW ROAD. BIKE TRAFFIC SHALL BE ALLOWED TO USE THE FULL LANE FOR TRAVEL.
 2. COMPLETE PAVEMENT SUB-BASE, CURB AND GUTTER, BITUMINOUS PAVING, SIDEWALK/RAMPS, AND FENCING. COMPLETE SEEDING. REINSTALL ALL SIGNAGE. COMPLETE FINAL RESTORATION AND WARRANTY WORK.
 3. MAINTAIN AT LEAST ONE LANE IN EACH DIRECTION ON VALLEY VIEW RD THROUGHOUT CONSTRUCTION
 4. RE-OPEN TRACY AVE. TO ALL TRAFFIC FROM VALLEY VIEW ROAD ROUNDABOUT SOUTH OF TH62 TO VALLEY VIEW ROAD TO THE NORTH OF TH62.

DRAWN BY:	SD, SS				
DESIGNER:	KM				
CHECKED BY:	WB				
DESIGN TEAM					
NO.	BY	DATE	REVISIONS		

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William J. Bauer

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 Date: 06/22/2020 Lic. No. 55680

EDINA,
 MINNESOTA

TRAFFIC CONTROL - TRACY AVE

FILE NO.
 ED150253

22
 32

PERMANENT PAVEMENT MARKING PLAN

NOTES & GUIDELINES

GENERAL INFORMATION:

THE ENGINEER'S INVOLVEMENT IN THE APPLICATION OF THE MATERIAL SHALL BE LIMITED TO FIELD CONSULTATION AND INSPECTION. THE CONTRACTOR WILL PLACE NECESSARY 'SPOTTING' AT APPROPRIATE POINTS TO PROVIDE HORIZONTAL CONTROL FOR STRIPING AND TO DETERMINE NECESSARY STARTING AND CUTOFF POINTS. LONGITUDINAL JOINTS, PAVEMENT EDGES AND EXISTING MARKINGS MAY SERVE AS HORIZONTAL CONTROL WHEN SO DIRECTED.

EDGE LINES AND LANE LINES ARE TO BE BROKEN ONLY AT INTERSECTIONS WITH PUBLIC ROADS AND AT PRIVATE ENTRANCES IF THEY ARE CONTROLLED BY AN AGENCY PLACED YIELD SIGN, STOP SIGN, OR TRAFFIC SIGNAL. THE BREAK POINT IS TO BE AT THE START OF THE RADIUS FOR THE INTERSECTION OR AT MARKED STOP LINES OR CROSSWALKS.

A TOLERANCE OF 1/4 INCH UNDER OR 1/4 INCH OVER THE SPECIFIED WIDTH WILL BE ALLOWED FOR STRIPING PROVIDED THE VARIATION IS GRADUAL AND DOES NOT DETRACT FROM THE GENERAL APPEARANCE. BROKEN LINE SEGMENTS MAY VARY UP TO 3 INCHES FROM THE SPECIFIED LENGTHS PROVIDED THE OVER AND UNDER VARIATIONS ARE REASONABLY COMPENSATORY. ALIGNMENT DEVIATIONS FROM THE CONTROL GUIDE SHALL NOT EXCEED 1 INCH. MATERIAL SHALL NOT BE APPLIED OVER LONGITUDINAL JOINTS. ESTABLISHMENT OF APPLICATION TOLERANCES SHALL NOT RELIEVE THE CONTRACTOR OF THEIR RESPONSIBILITY TO COMPLY AS CLOSELY AS PRACTICABLE WITH THE PLANNED DIMENSIONS.

JUST PRIOR TO THE PLACEMENT OF PAVEMENT MARKINGS THE ROAD SURFACE SHALL BE CLEANED AND FREE OF CONTAMINATION AS RECOMMENDED BY THE MATERIAL MANUFACTURER AND ACCEPTABLE TO THE ENGINEER. PORTLAND CEMENT CONCRETE SURFACES SHALL BE SANDBLAST CLEANED TO REMOVE ANY SURFACE TREATMENTS AND/OR LAITANCE.

APPLY ALL PAVEMENT MARKINGS AS RECOMMENDED BY THE MATERIAL MANUFACTURER.

PERMANENT PAVEMENT MARKINGS SHALL NOT BE PLACED OVER TEMPORARY TAPE MARKINGS.

THE FILLING OF TANKS, POURING OF MATERIALS OR CLEANING OF EQUIPMENT SHALL NOT BE PERFORMED ON UNPROTECTED PAVEMENT SURFACES UNLESS ADEQUATE PROVISIONS ARE MADE TO PREVENT SPILLAGE OF MATERIAL.

PREFORMED MARKINGS:

MANUFACTURER CERTIFICATIONS ARE REQUIRED FOR INSTALLERS AND WRITTEN CERTIFICATION SHALL BE PRESENTED AT ANYTIME UPON THE REQUEST OF ENGINEER OR OTHER STATE PERSONNEL.

DO NOT USE LINE MATERIAL TO PIECE TOGETHER INDIVIDUAL LETTERS, SYMBOLS OR CROSSWALK BLOCKS. UTILIZE PRECUT KITS PROVIDED BY THE MANUFACTURER. TWO STRIPS OF 18" LINE MATERIAL MAY BE USED TO FORM CROSSWALK BLOCKS OF 36" WIDTH.

DO NOT USE NARROWER LINE MATERIAL TO PIECE TOGETHER WIDER LANES.

IF THERE IS A CRACK OR JOINT IN ROAD SURFACE (FOR TAPE LAY OVER CRACK OR JOINT THEN CUT TAPE 1" ON EACH SIDE OF CRACK OR JOINT). (FOR THERMO MAKE A DEEP SCORE IN THE MATERIAL ONCE IT HAS SET UP BUT NOT ENTIRELY COOLED DOWN).

PREFORMED MARKINGS GROUND IN APPLICATION:

CONCRETE PAVEMENT SURFACES AND BITUMINOUS PAVEMENT SURFACES WHERE PAVEMENT MARKINGS CANNOT BE INLAID IN THE HOT MAT, SHALL HAVE A RECESS GROUND IN FOR THE PLACEMENT OF DURABLE REFLECTORIZED PAVEMENT MARKINGS. SEE CONSTRUCTION SPECIFICATIONS.

PAINT:

GLASS BEADS SHALL BE APPLIED AT A RATE OF AT LEAST 8 LBS/GAL. IMMEDIATELY AFTER APPLICATION OF THE PAINT LINE.

PAVEMENT MARKINGS SHALL ONLY BE APPLIED IN SEASONABLE WEATHER WHEN AIR AND PAVEMENT SURFACE TEMPERATURES IS 50°F OR HIGHER AND SHALL NOT BE APPLIED WHEN THE WIND OR OTHER CONDITIONS CAUSE A FILM OF DUST TO BE DEPOSITED ON THE PAVEMENT SURFACE AFTER CLEANING AND BEFORE THE MARKING MATERIAL CAN BE APPLIED.

MULTI COMPONENT:

THE ROAD SURFACE SHALL BE CLEANED AT THE DIRECTION OF THE ENGINEER JUST PRIOR TO APPLICATION. PAVEMENT CLEANING SHALL CONSIST OF AT LEAST BRUSHING WITH A ROTARY BROOM (NON-METALLIC) OR AS RECOMMENDED BY THE MATERIAL MANUFACTURER AND ACCEPTABLE TO THE ENGINEER. NEW PORTLAND CEMENT CONCRETE SURFACES SHALL BE SANDBLAST CLEANED TO REMOVE ANY SURFACE TREATMENTS AND/OR LAITANCE.

THE MULTI COMPONENT MARKING APPLICATION SHALL IMMEDIATELY FOLLOW THE PAVEMENT CLEANING. GLASS BEADS SHALL BE APPLIED IMMEDIATELY AFTER APPLICATION OF THE MULTI COMPONENT RESIN LINE.

APPLY MULTI COMPONENT MARKINGS WITH A MINIMUM THICKNESS OF 20 MILS. GLASS BEADS SHALL BE APPLIED AT A RATE OF AT LEAST 25 LB/GAL. THE "NO-TRACKING" CONDITION SHALL BE DETERMINED ON AN APPLICATION OF SPECIFIED THICKNESS TO THE PAVEMENT AND COVERED WITH GLASS BEADS AT THE RATE OF AT LEAST 25 LB/GAL.

PAVEMENT MARKINGS SHALL ONLY BE APPLIED IN SEASONABLE WEATHER WHEN AIR AND PAVEMENT SURFACE TEMPERATURES ARE 40°F OR HIGHER AND SHALL NOT BE APPLIED WHEN THE WIND OR OTHER CONDITIONS CAUSE A FILM OF DUST TO BE DEPOSITED ON THE PAVEMENT SURFACE AFTER CLEANING AND BEFORE THE MARKING MATERIAL CAN BE APPLIED.

SYMBOLS & MATERIALS LEGEND



PAVEMENT MESSAGE: (BIKE LANE WITH ARROW) – THERMOPLASTIC GROUND IN



CROSSWALK MARKINGS: (WHITE ON BLACK) THERMOPLASTIC GROUND IN

STRIPING KEY

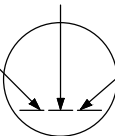


CIRCLE–MULTI COMPONENT GR–IN

1ST DIGIT
WIDTH
4", 8", ETC.

2ND DIGIT
PATTERN
S – SOLID
B – BROKEN
T – DOTTED
D – DOUBLE
K – DOUBLE BROKEN
H – DOUBLE DOTTED

3RD DIGIT
COLOR
W – WHITE
Y – YELLOW
B – BLACK


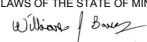


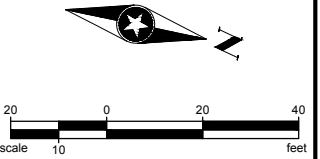
EXAMPLE: (4SW) = 4" SOLID LINE MULTI COMPONENT GR–IN WHITE

PAVEMENT MARKING TABULATION

ITEM		UNIT	TOTALS
2581.503	REMOVABLE PREFORM PAVEMENT MARKING TAPE - YELLOW	LIN FT	1,400
2581.518	REMOVABLE PREFORM PLASTIC MASK (BLACK)	SQ FT	288
2582.503	4-INCH DOUBLE SOLID LINE YELLOW MULTI COMP GR IN (WR)	LIN FT	1,282
2582.503	4-INCH SOLID LINE WHITE MULTI COMP GR IN (WR)	LIN FT	56
2582.503	6-INCH SOLID LINE WHITE MULTI COMP GR IN (WR)	LIN FT	1,181
2582.503	24-INCH SOLID LINE YELLOW MULTI COMP GR IN (WR)	LIN FT	312
2582.518	CROSSWALK PREFORM THERMOPLASTIC GROUND IN	SQ FT	90
2582.602	PAVEMENT MESSAGE PREFORM THERMOPLASTIC GROUND IN - BIKE LANE WITH ARROW	EACH	6

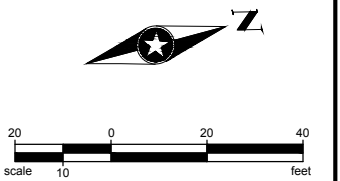
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DESIGNER: KM											
CHECKED BY: WB											
DESIGN TEAM	NO.	BY	DATE	REVISIONS							





KEYED NOTES

(A) SALVAGE AND REINSTALL SIGN

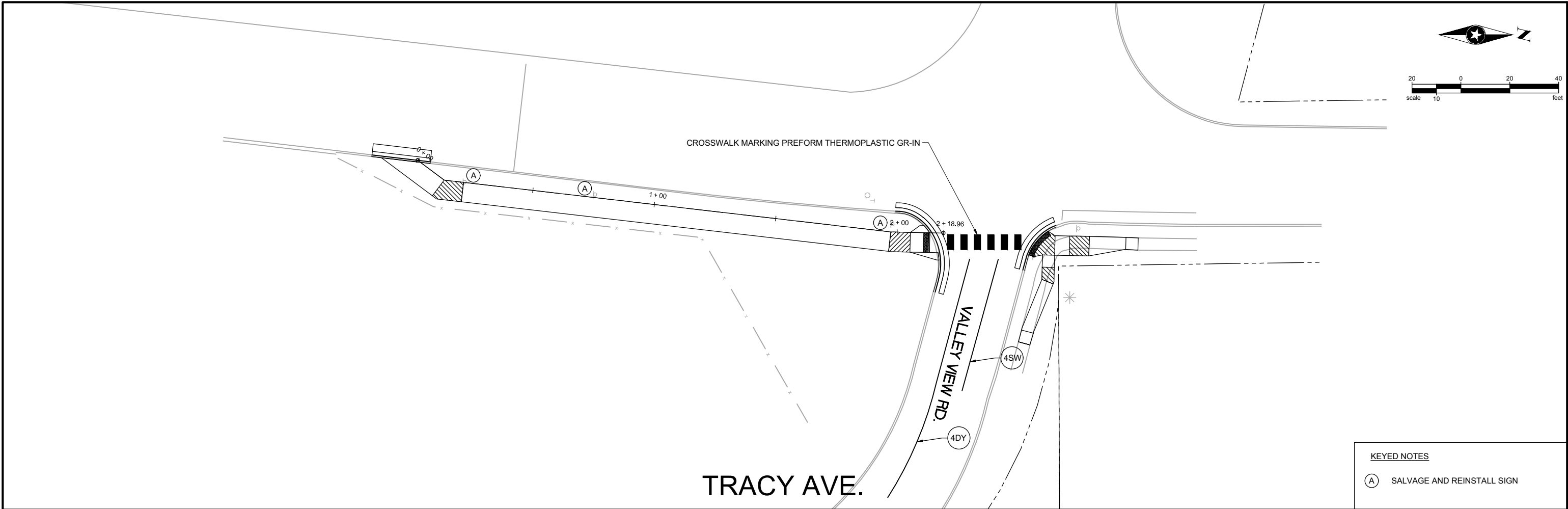


KEYED NOTES

(A) SALVAGE AND REINSTALL SIGN

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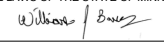


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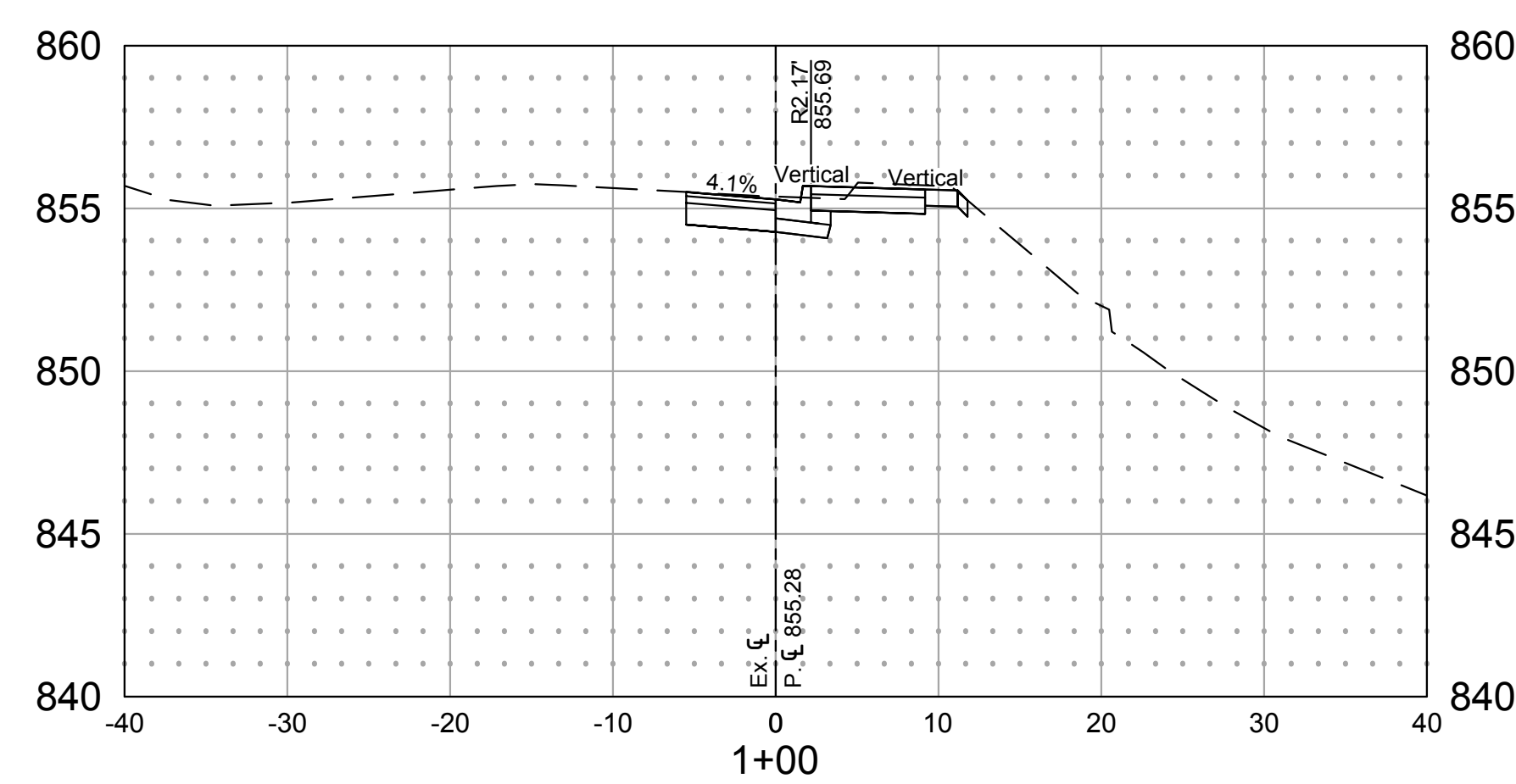
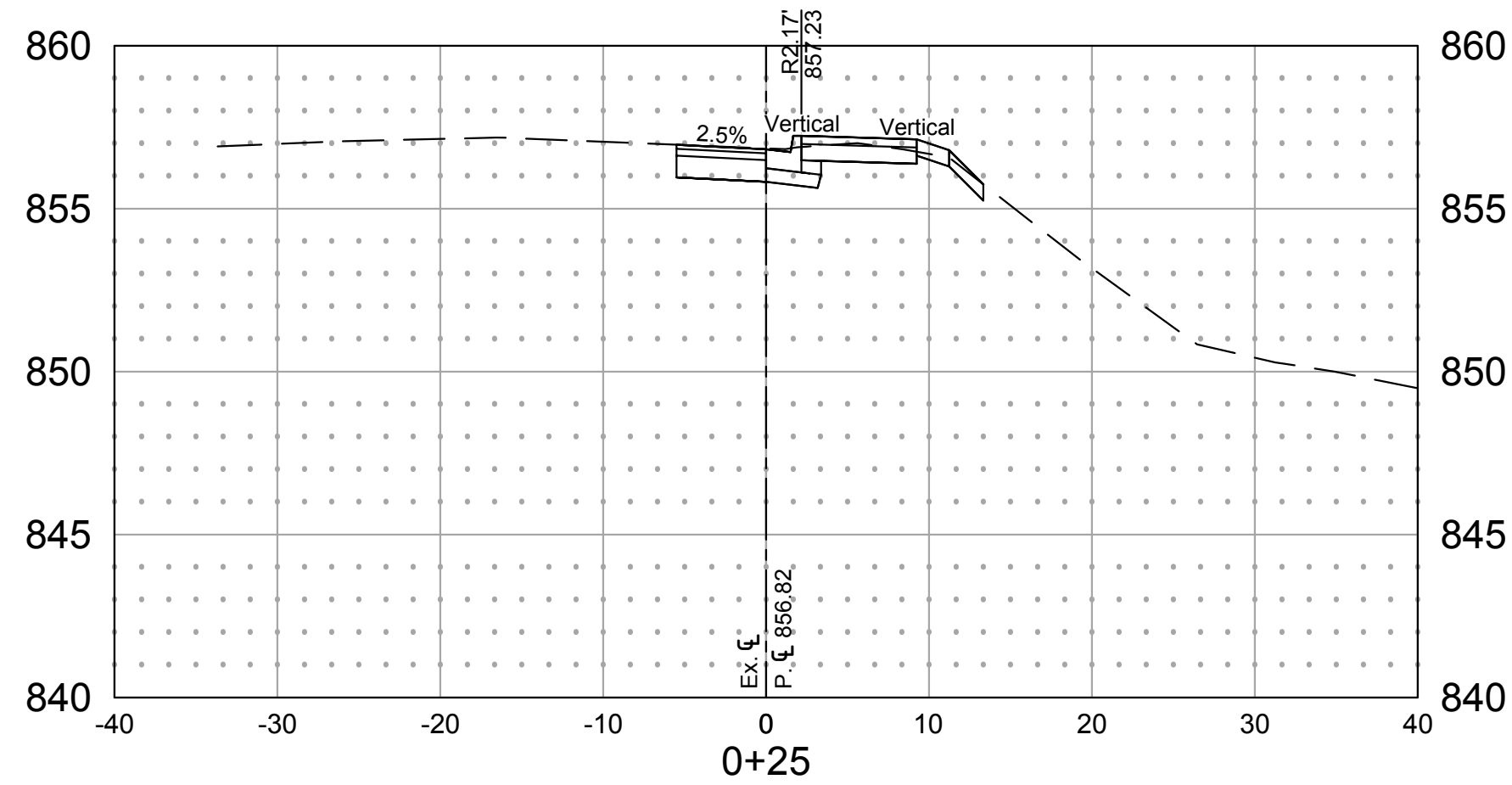
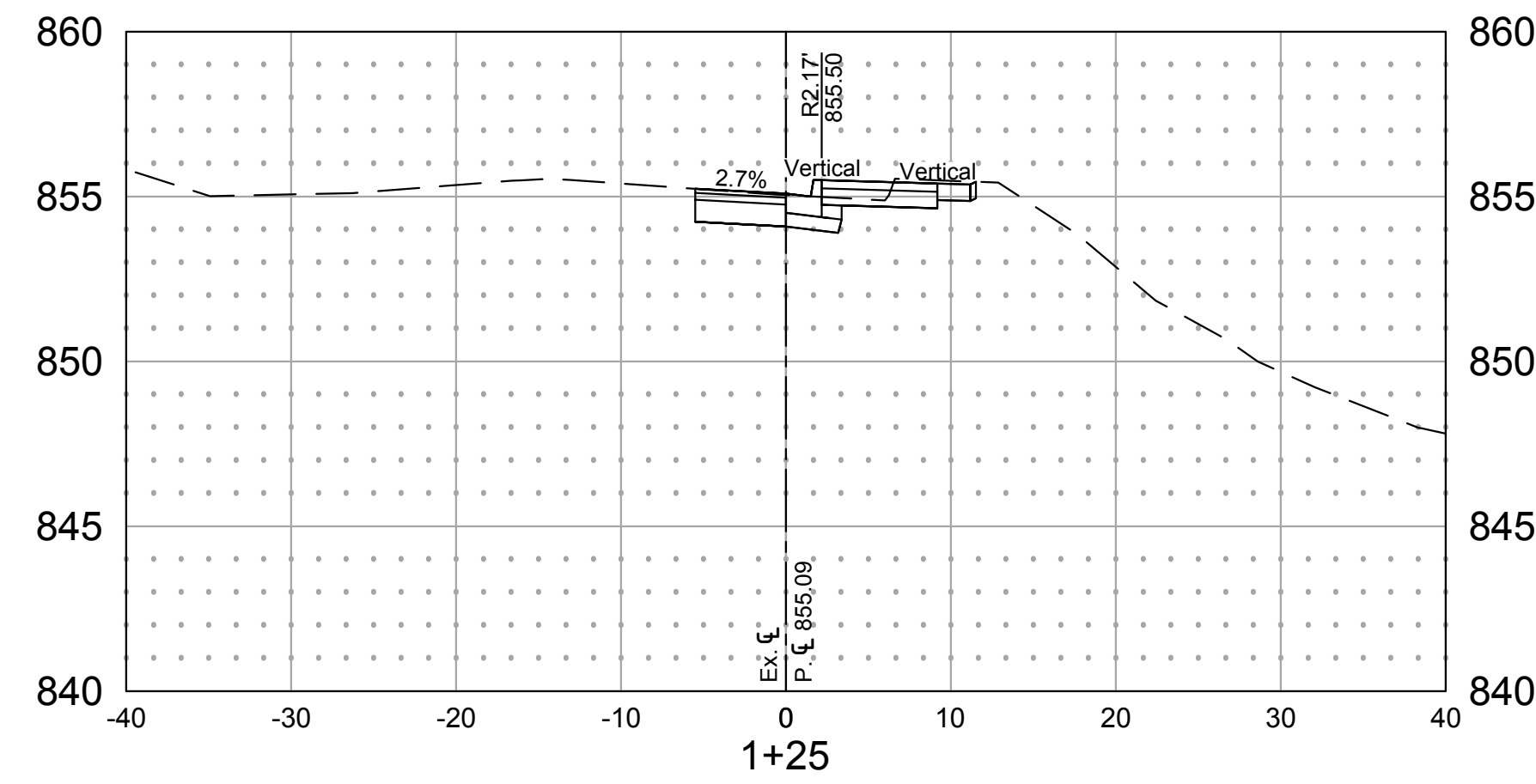
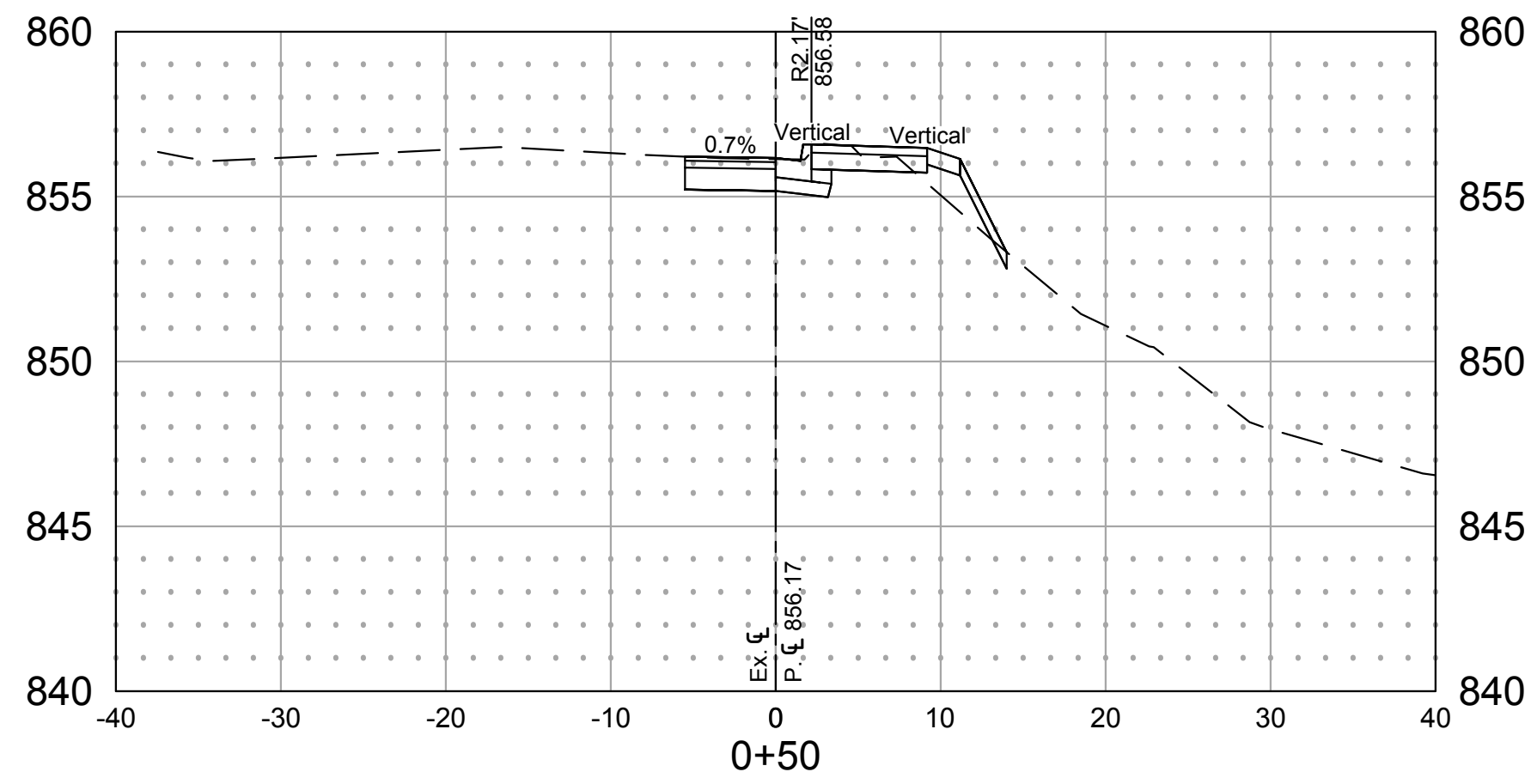
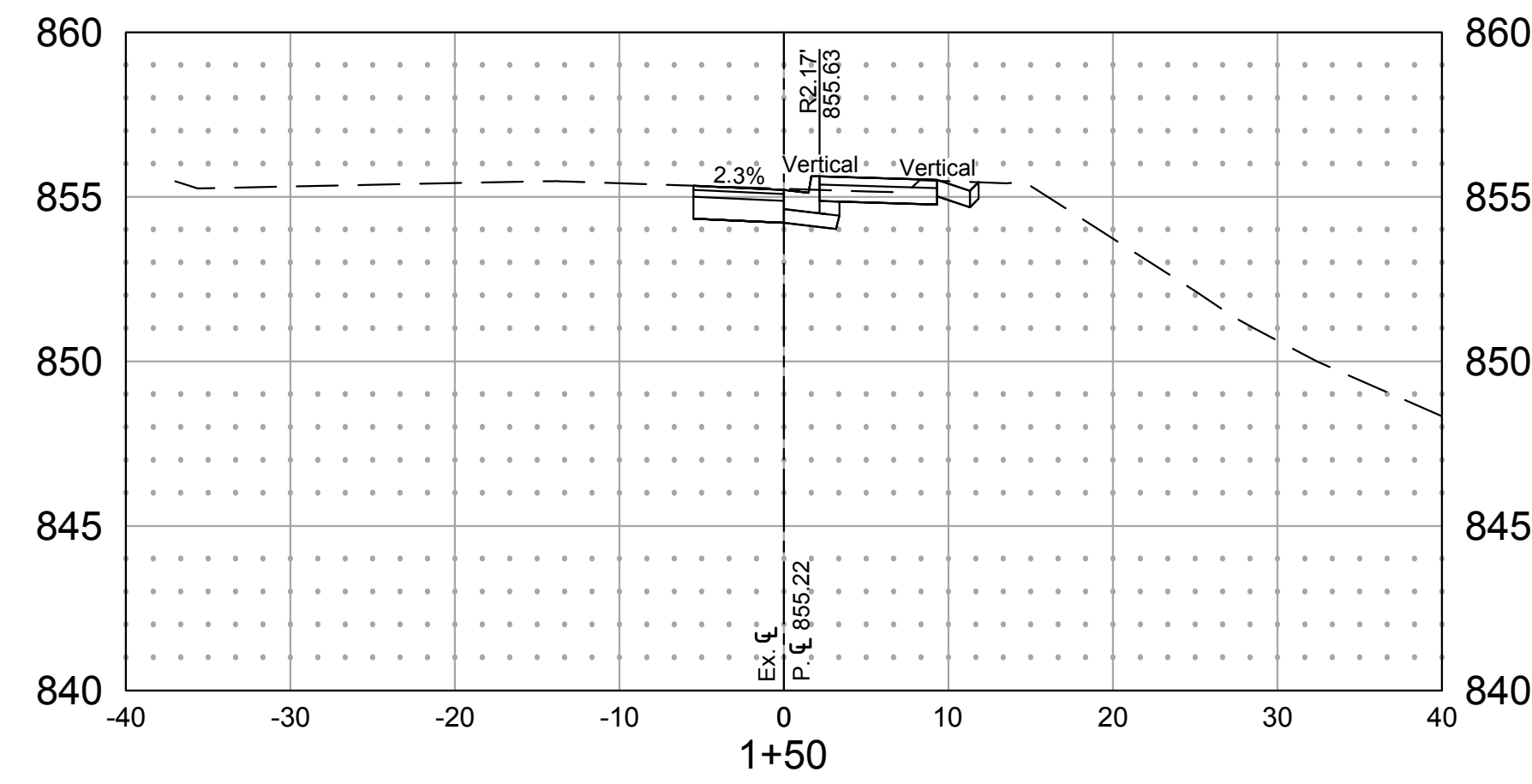
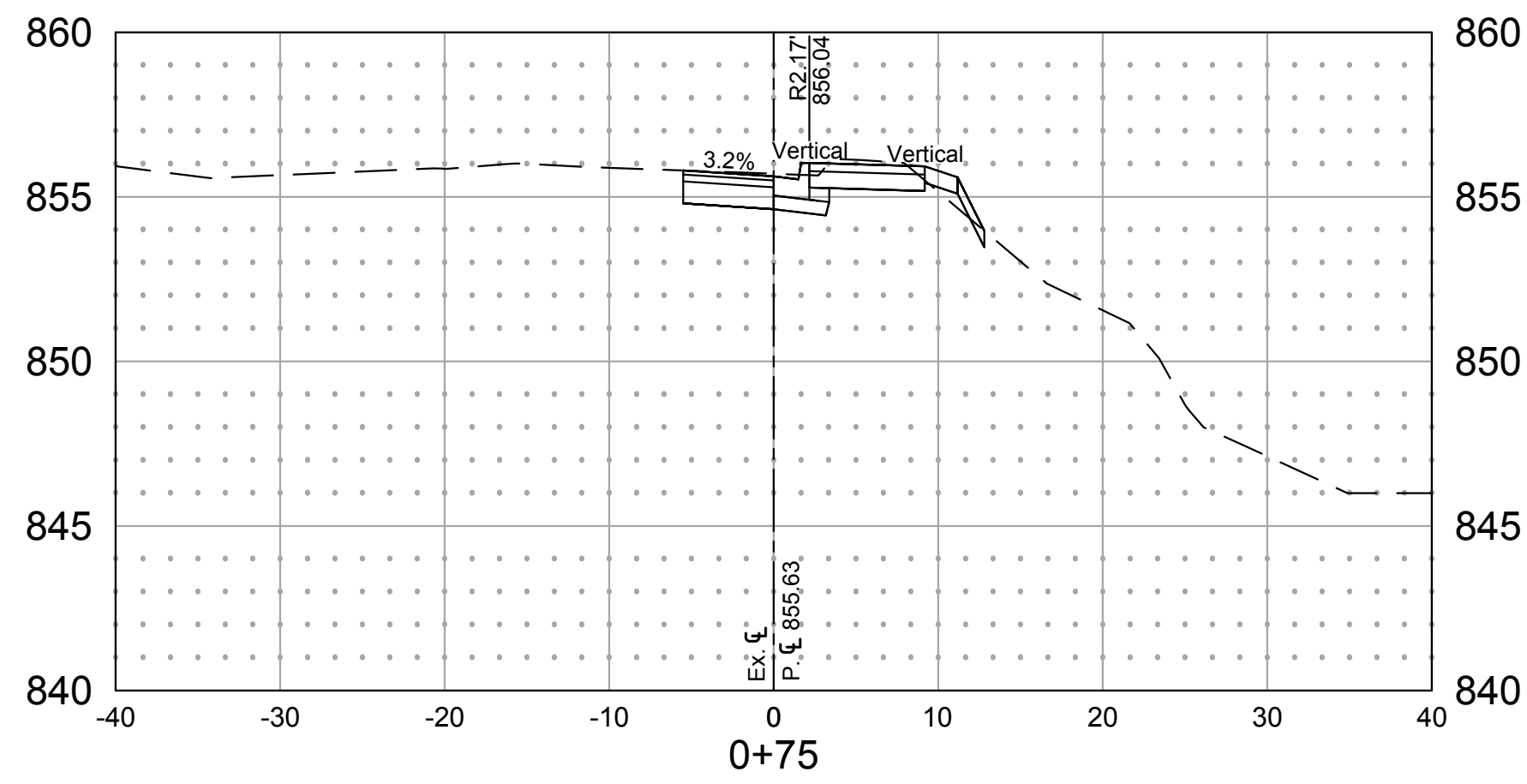
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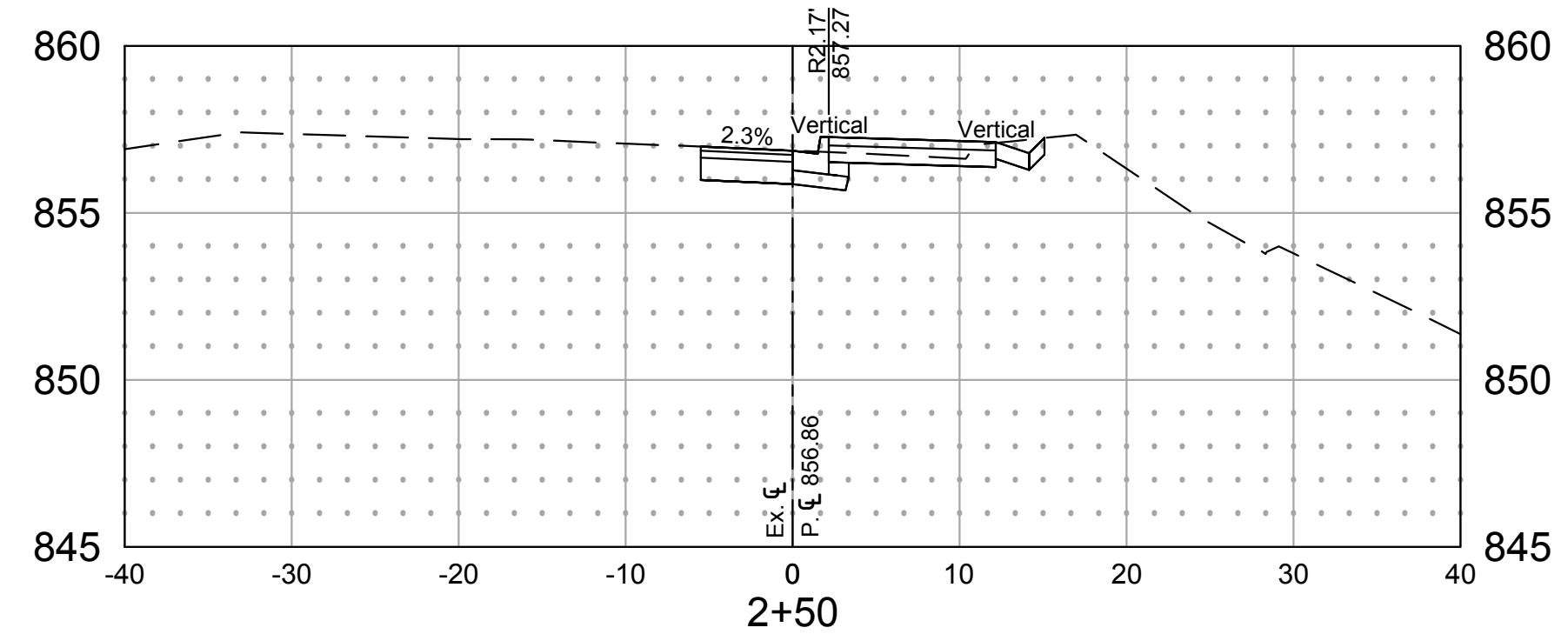
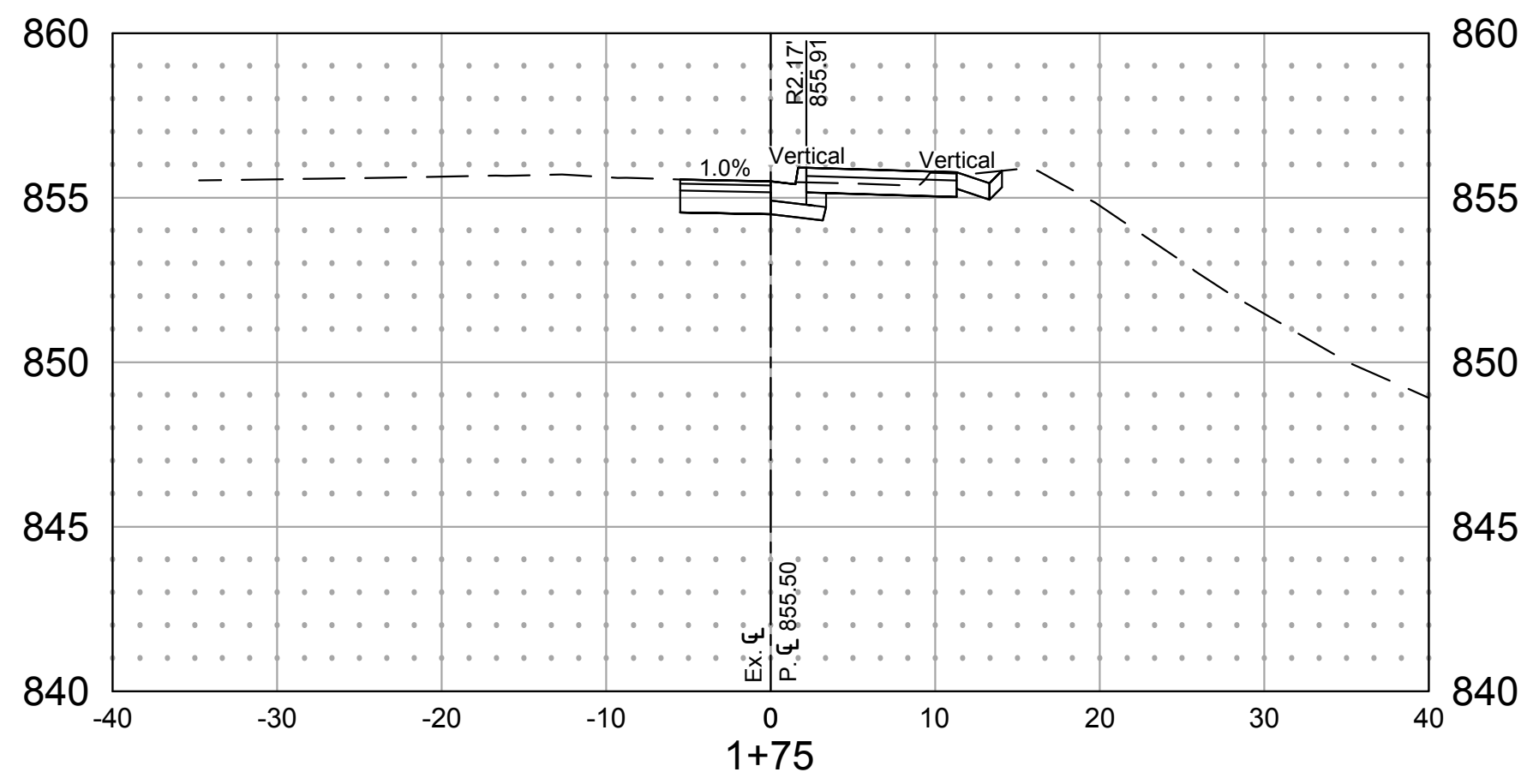
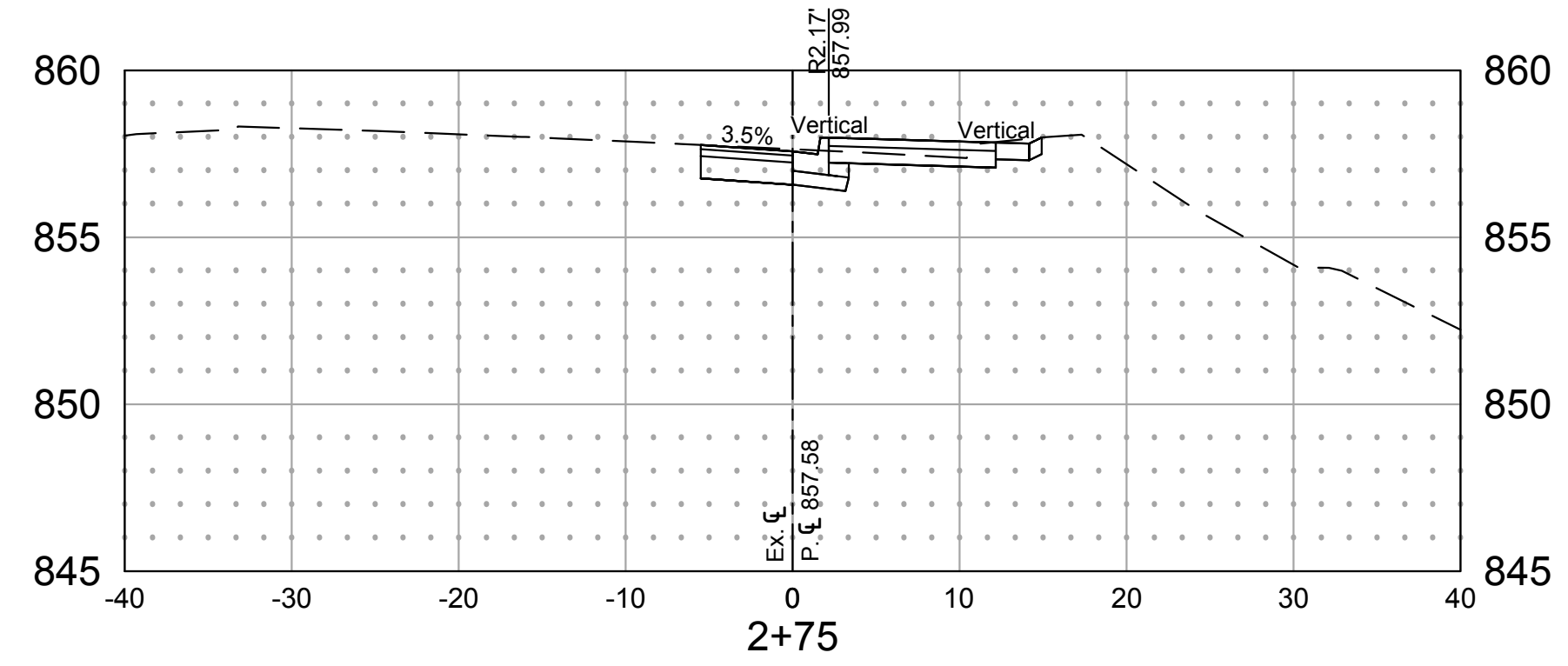
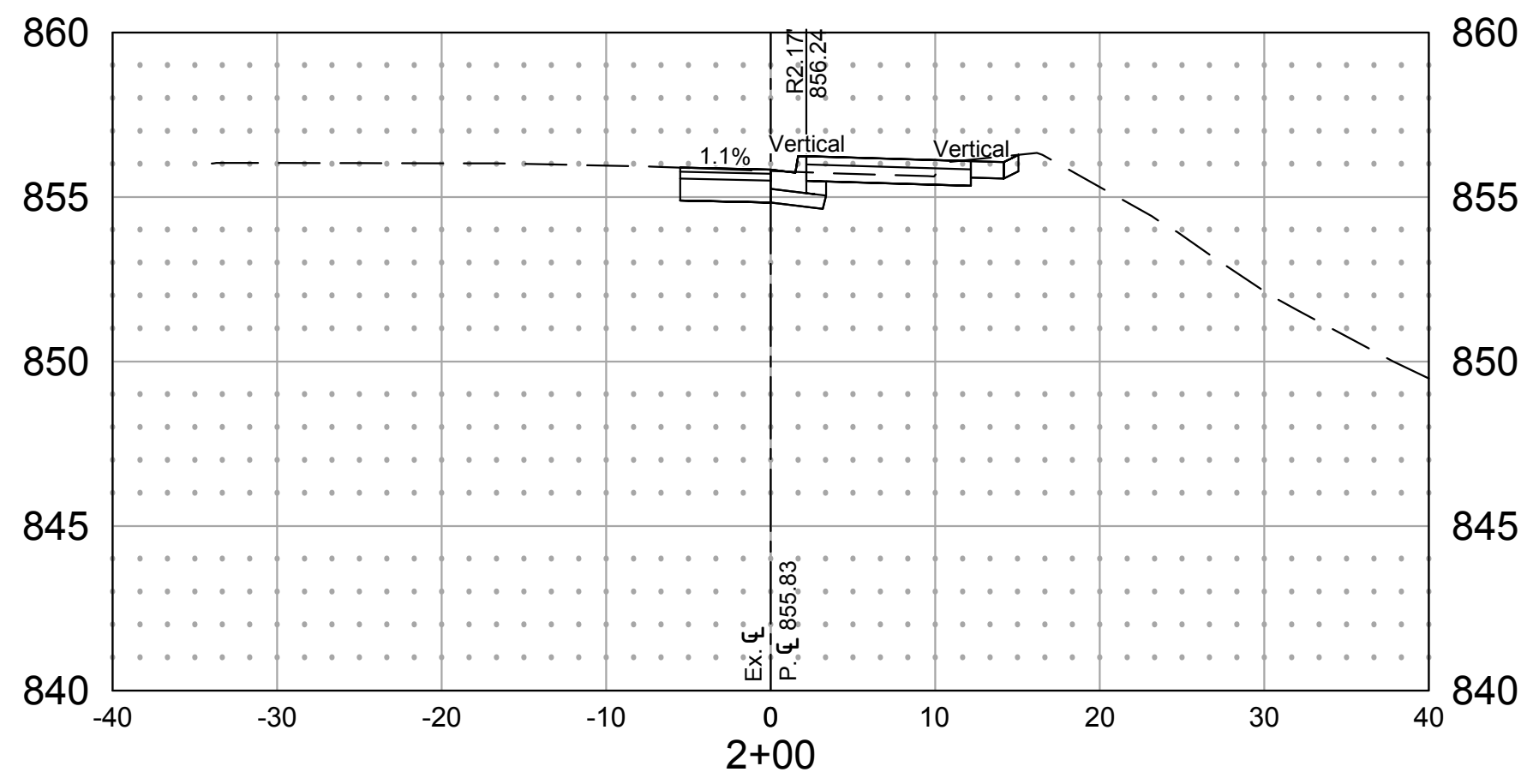
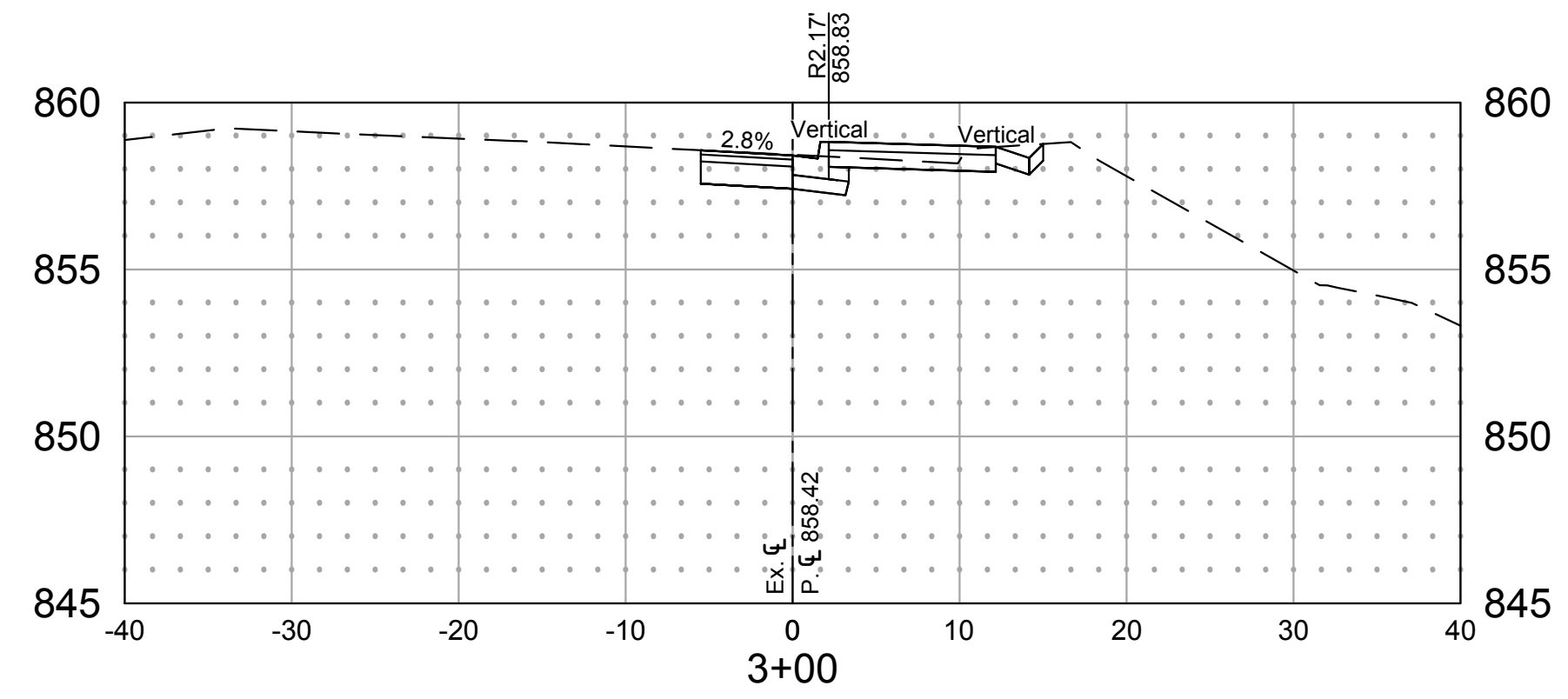
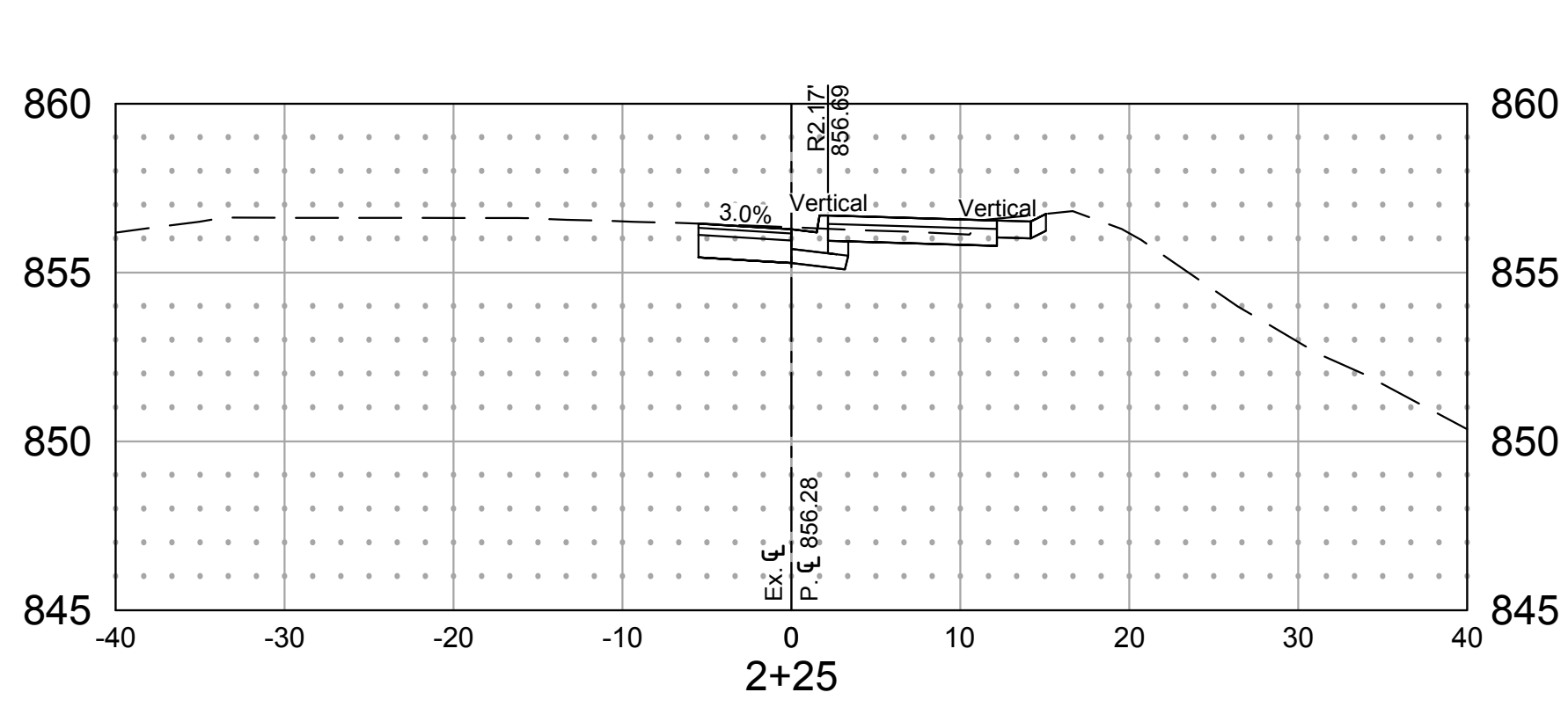
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TRACY AVE.

FILE NO.
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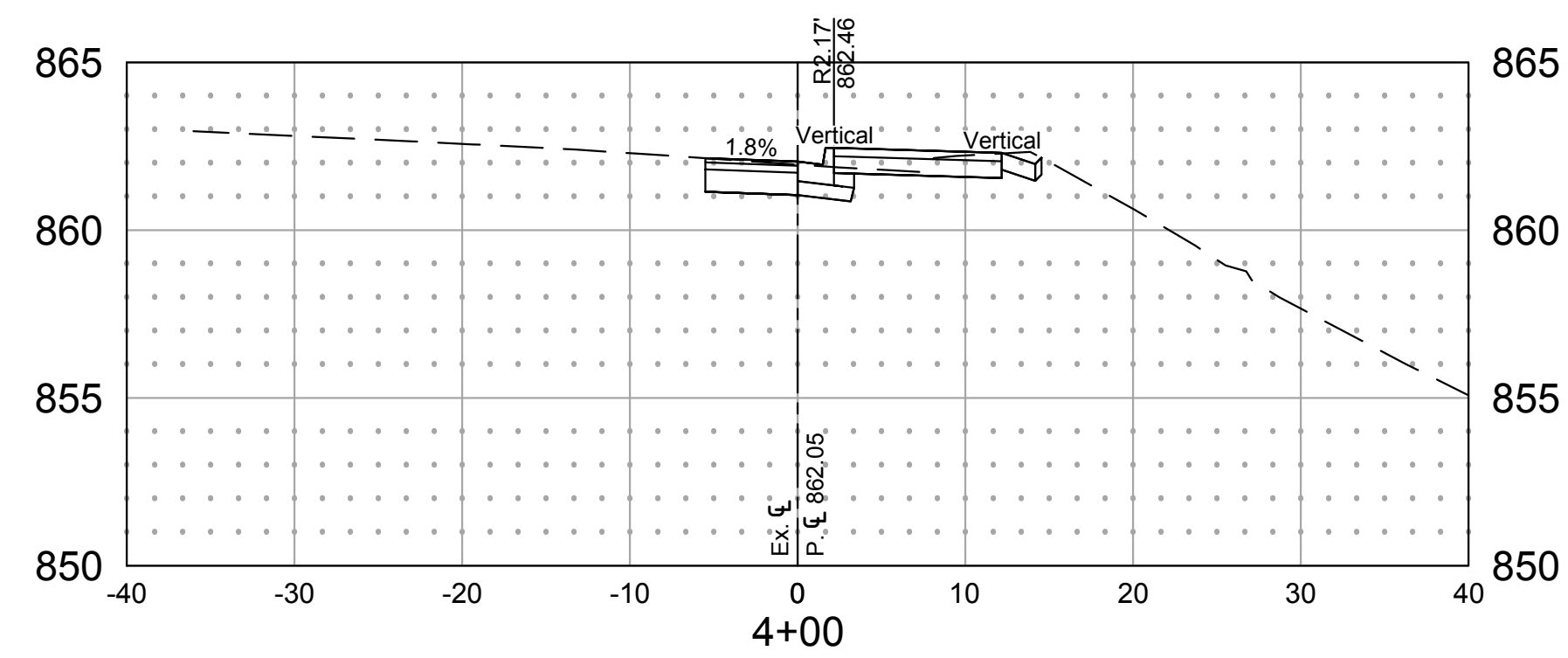
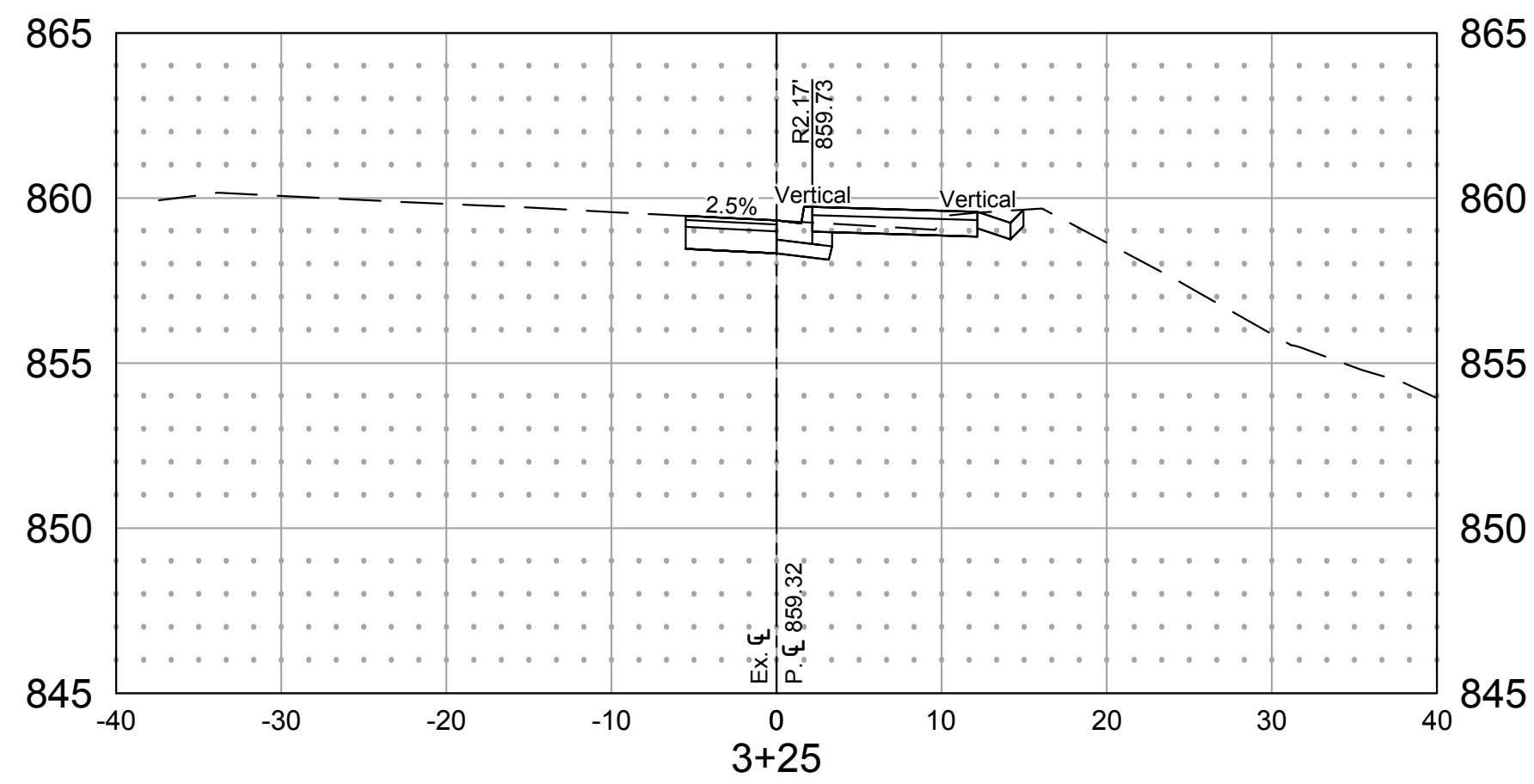
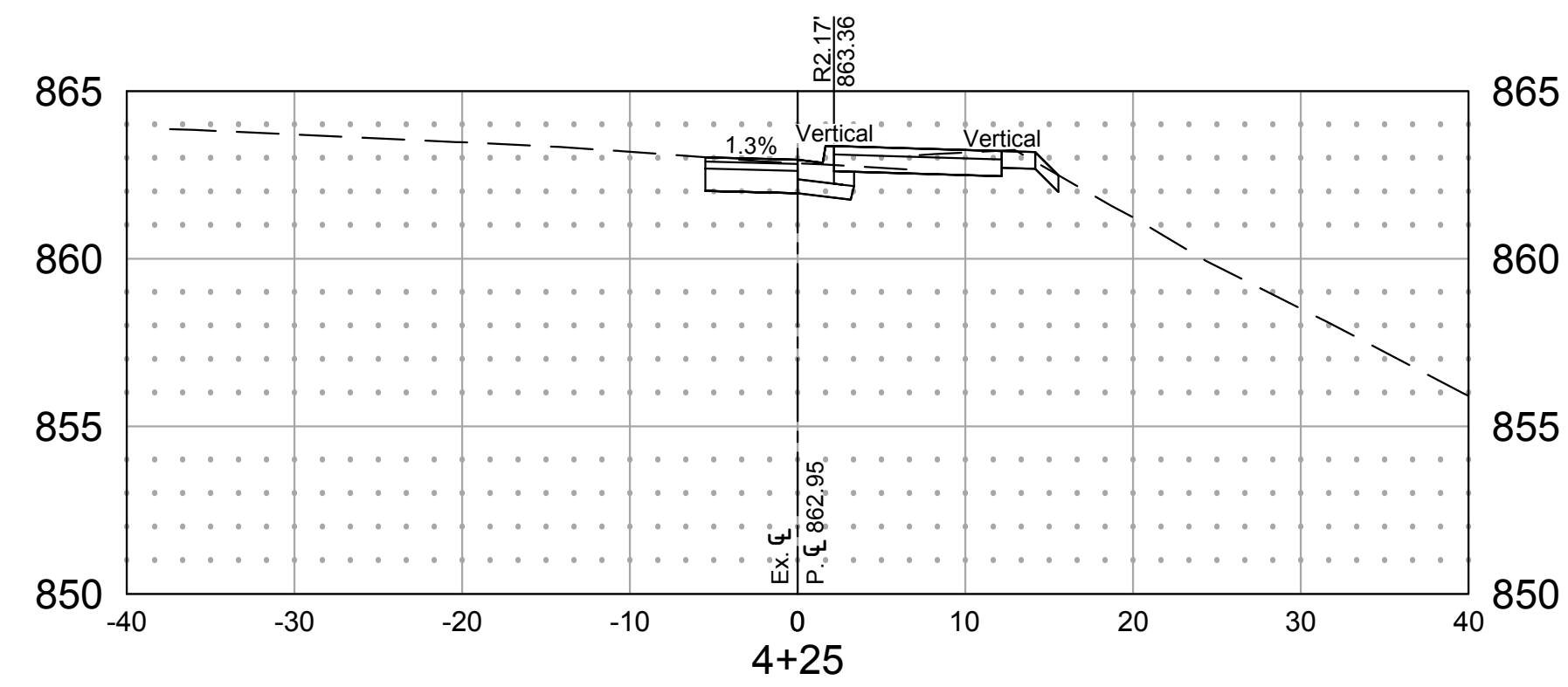
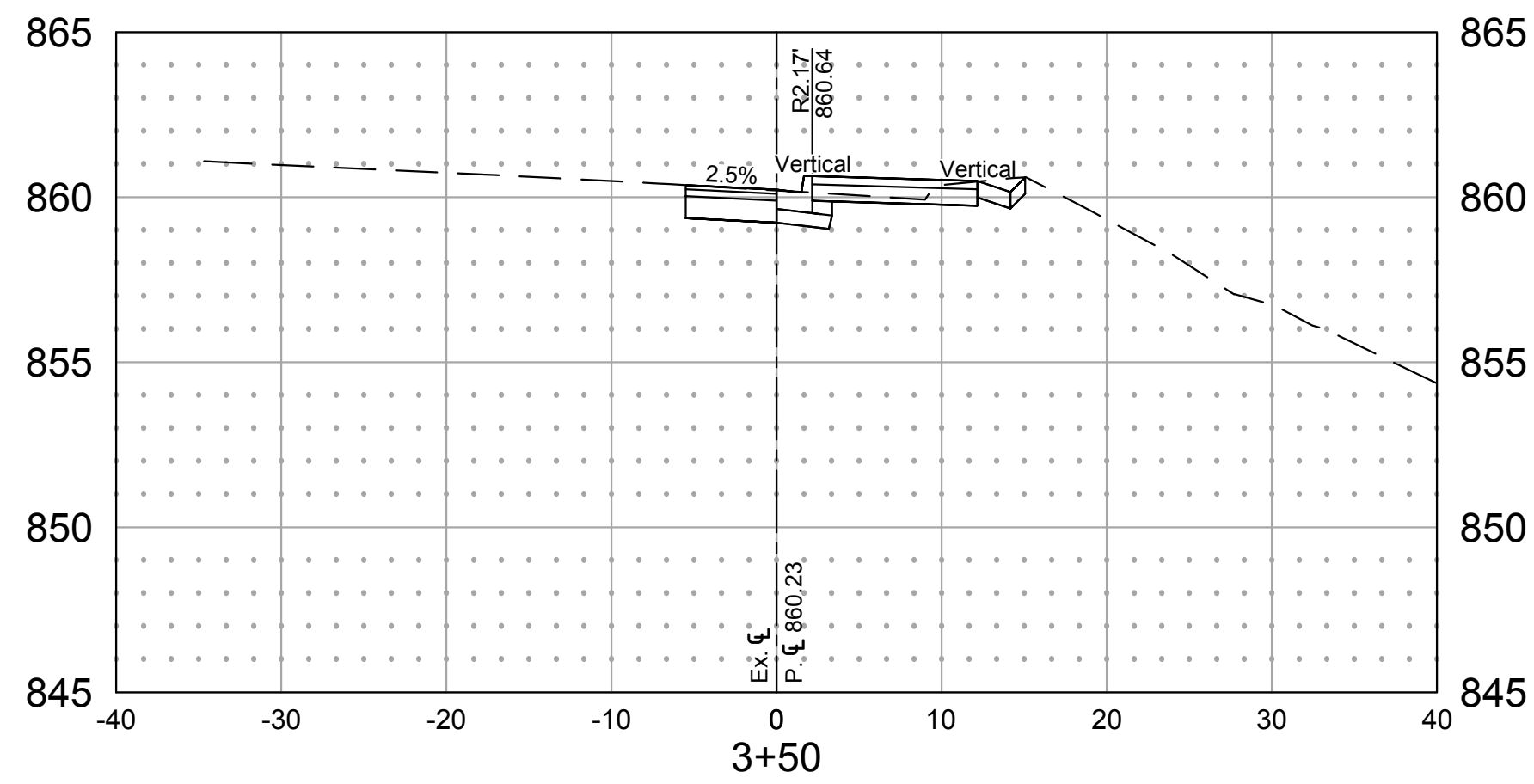
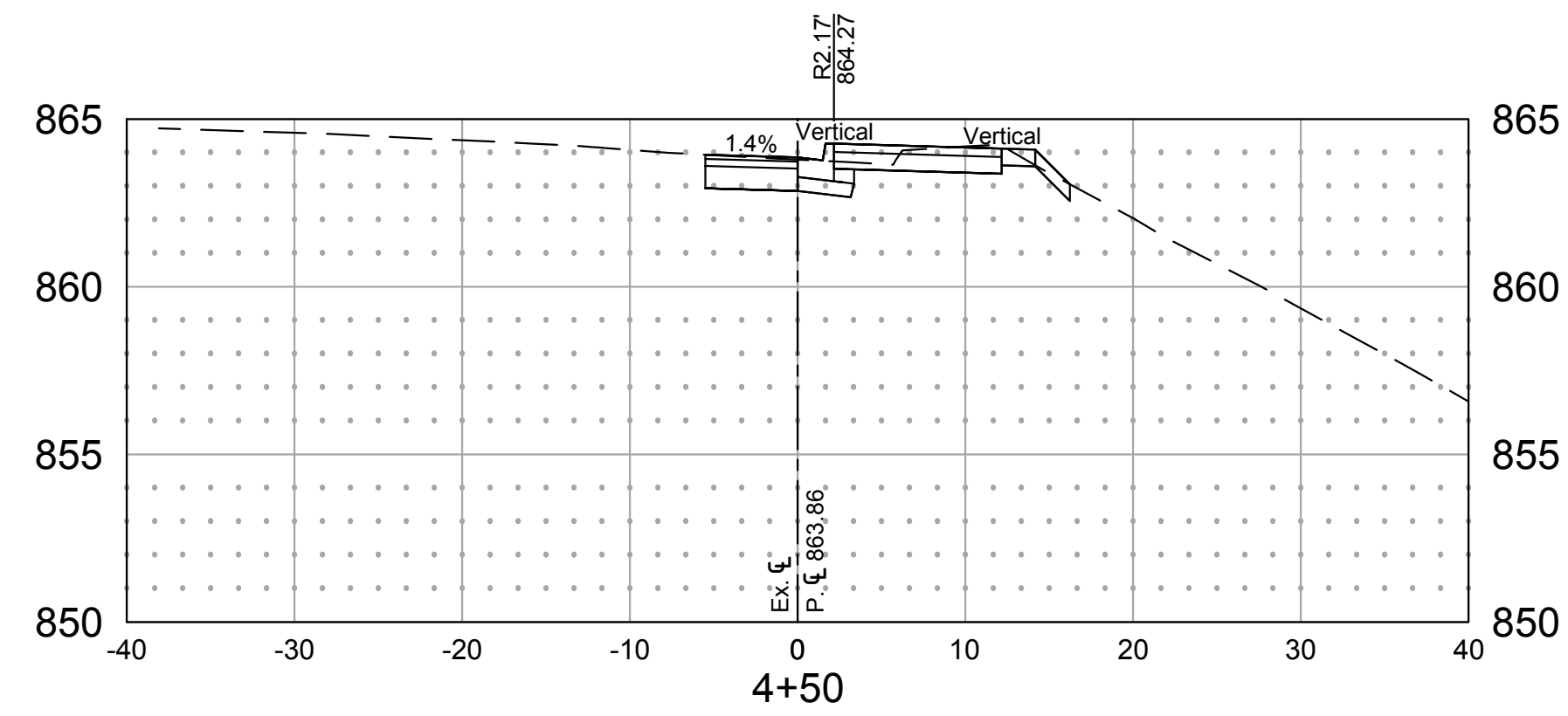
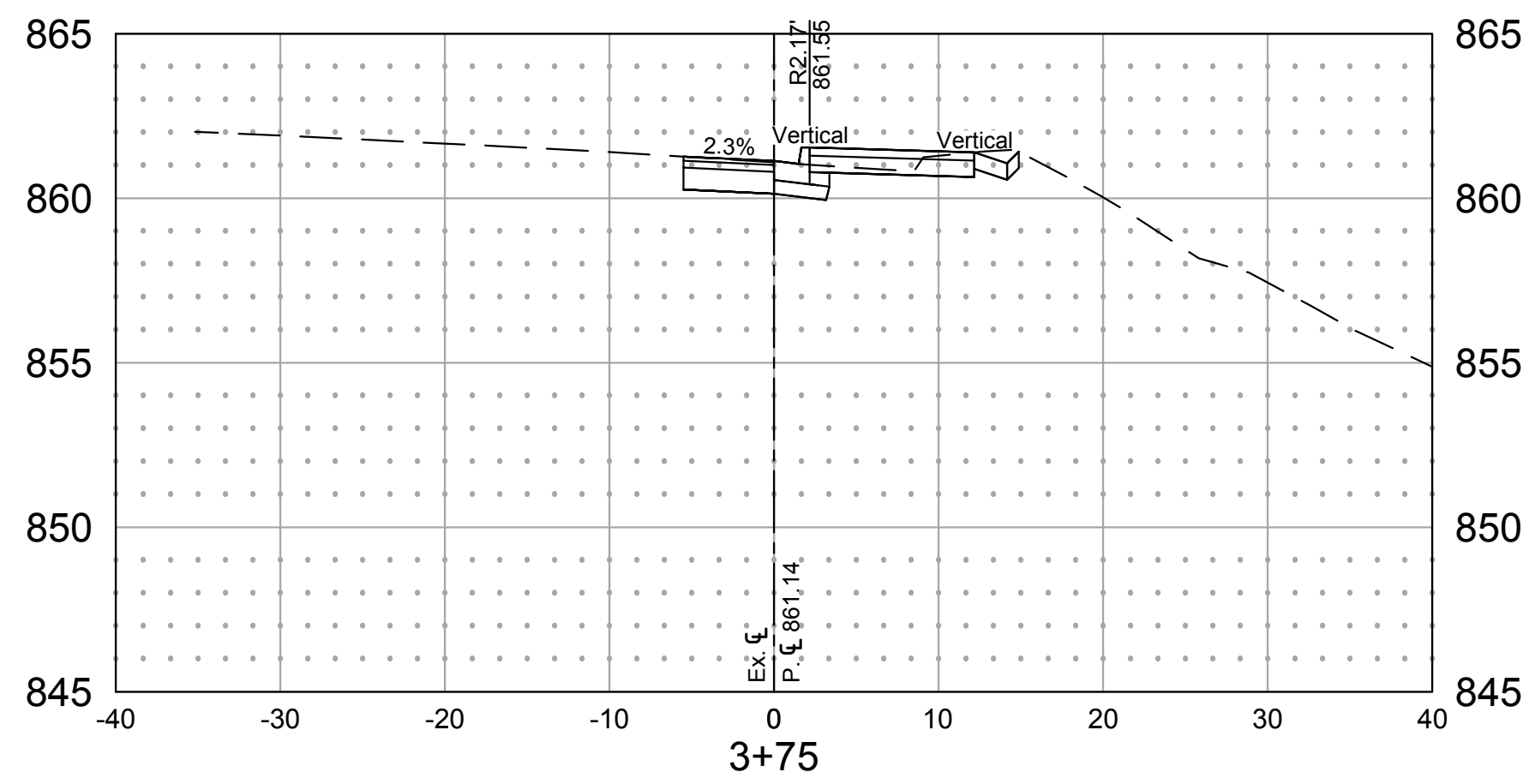
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